

3.0 PROJECT DESCRIPTION

The purpose of this section of the Draft Environmental Impact Report (EIR) is to describe the proposed Dignity Health North State Pavilion Project (UP-2017-00001, PM-2017-00002, GPA-2017-00003, RZ-2017-00004) (herein referenced as the proposed project) in a useful and comprehensible manner to the public, other organizations, agencies, and decision-makers. State *CEQA Guidelines* §15124 requires a project description to contain:

- *A regional map showing the location of the project and a detailed map showing the precise location and boundaries of the project;*
- *A statement of objectives sought by the project, including the underlying purpose of the project;*
- *A general description of the project's technical, economic, and environmental characteristics; and*
- *A statement briefly describing the intended uses of the EIR, including a list of agencies that are expected to use the EIR in their decision-making, a list of permits and other approvals required to implement the project, and a list of related environmental review and consultation requirements required by Federal, State, and local laws, regulations or policies.*

Under CEQA, the project description is required to provide general information but not an engineering level of detail. The State *CEQA Guidelines* provide: *"The description of the project shall contain the following information but should not supply extensive detail beyond that needed for evaluation and review of the environmental impact.*

- *A general description of the project's technical, economic, and environmental characteristics, considering the principal engineering proposals if any and supporting public service facilities."* (State *CEQA Guidelines* §15124[c].)

The information provided in this EIR section meets the requirements of State *CEQA Guidelines* §15124 and provides a level of detail adequate for public, other organization and agency review and consideration of the proposed project and the potential environmental impacts associated with project implementation.

3.1 PROJECT LOCATION AND SETTING

REGIONAL SETTING

The City of Redding is located in central Shasta County in northern California, on the north end of the Central Valley, approximately 155 miles north of the City of Sacramento and approximately 215 miles northeast of the City of San Francisco. The proposed project is located within a developed area in southeast Redding designated in the City of Redding *2000-2020 General Plan* (herein referenced as the *General Plan*) as "General Office" (GO), "General Commercial" (GC), and "Greenway" (GWY), and is zoned "General Office" (GO), "General Commercial" (GC), and "Open Space" (OS). Southeast Redding includes all the area within the planning area west of the Sacramento River and south of State Route 44 (SR-44) (refer to Figure 3-1, SITE VICINITY).

PROJECT LOCATION

The proposed project area is generally located at the southwest corner of the Cypress Avenue and Hartnell Avenue intersection (refer to Figure 3-2, AERIAL PHOTOGRAPH). The project area is bounded on the west by the Henderson Open Space, the eastern and a portion of the southern boundary is generally defined by an existing primarily vacant shopping center, referenced as the Cobblestone (formerly Raley's) Shopping Center; on the north by Cypress Avenue bridge; the southwest by two vacant lots; and the south by Parkview Avenue. Access to the proposed project site from the north is via Hartnell Avenue to Henderson Road (North) or from the south via Hartnell Avenue, then onto Parkview Avenue to the west and then north via Henderson Road (South). The distance to Interstate 5 (I-5) to the east is approximately one-half mile and to the downtown core, to the west, approximately 1.3 miles.

EXISTING ONSITE CONDITIONS

The proposed project encompasses approximately 10.55 acres and is currently partially developed with two small buildings, foundation remnants, gravel surfaced areas, and undeveloped vacant land. Site topographic elevations range from approximately 497 feet above mean sea level (msl) in the northeastern corner of the site adjacent to Hartnell Avenue to approximately 467 feet msl in the southern portion of the site around Parkview Avenue (South). Overall, the site slopes in a western direction towards the Sacramento River with slopes varying between 1.7 and 3.3 percent (refer to Figure 3-3, USGS).

Onsite plant communities/wildlife habitats consist of a riparian woodland, annual grassland, and urban habitat; individual or small stands of valley oaks, interior live oaks, blue oaks, and tree of heaven are present in places; however, they do not form a distinct oak woodland community. Field inspection confirmed that no special-status plant species are present, nor are any expected to be present. Onsite habitats may support several special-status animal species, including pallid bat, spotted bat, Townsend's big-eared bat, western red bat, and bald eagle. The Sacramento River, located just west of the site, supports Chinook salmon (fall-run, late-fall-run, winter-run, and spring-run) and Central Valley steelhead; the river reach is designated as critical habitat for Chinook salmon (winter-run and spring-run) and Central Valley steelhead. Field inspection confirmed that no wetlands are present onsite.

The Federal Emergency Management Agency (FEMA) 100-year floodplain of the Sacramento River inundates approximately 3.58 acres of the project site. Of this area, approximately 2.35 acres is within the FEMA mapped floodway limits, and the remaining 1.23 acres is within the FEMA mapped flood fringe. A proposed Letter of Map Revision (LOMR) has been submitted to FEMA proposing removal of the floodway from the project site and leaving approximately 1.88 acres of the site within FEMA's 100-year floodplain.

SURROUNDING LAND USES

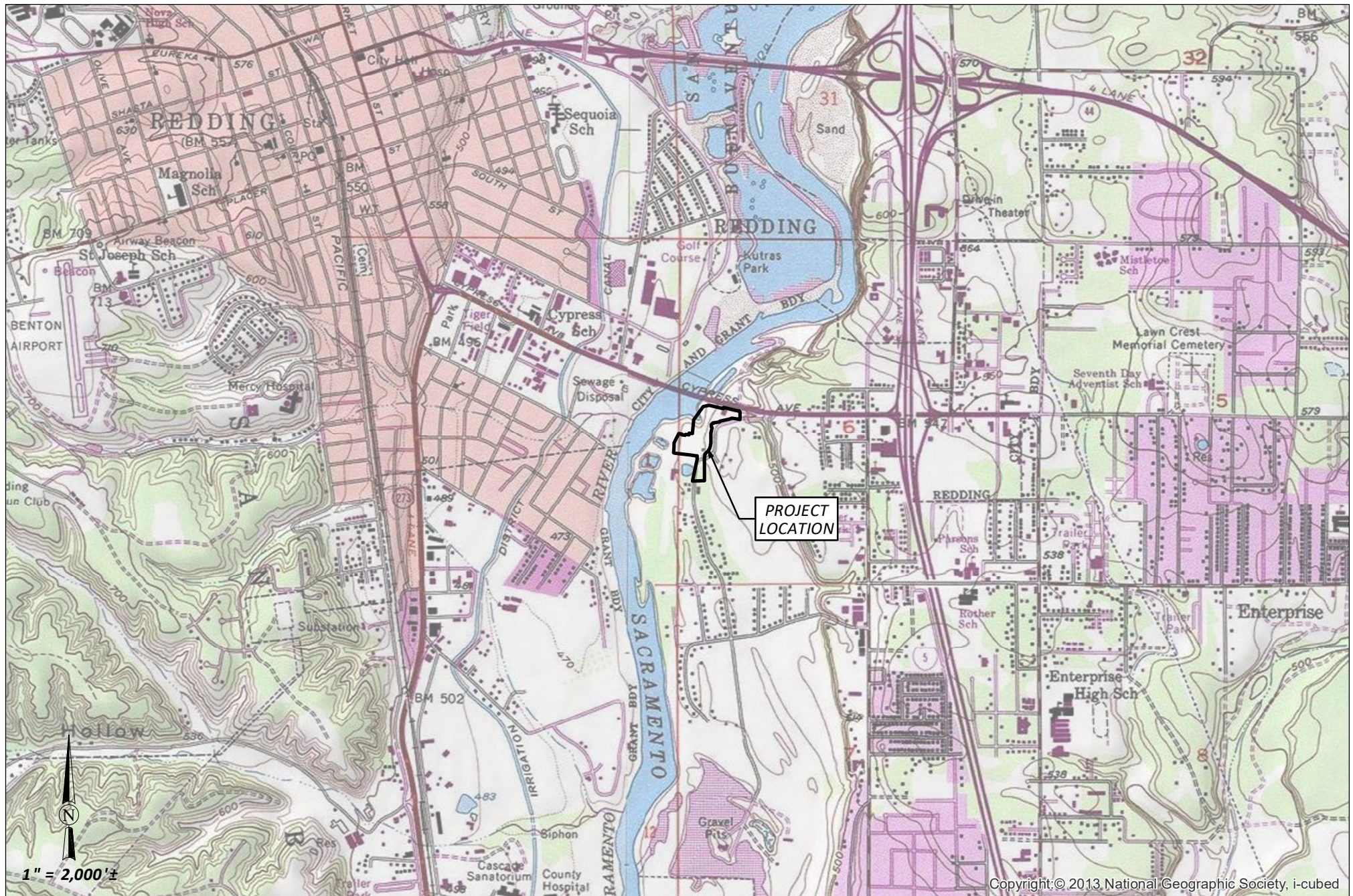
Commercial shopping center development represents the predominate land uses immediately east and southeast the proposed project with Hartnell Avenue abutting those uses. Offices are located further south of the shopping center. Two undeveloped parcels of 4.42 acres and 2.10 acres are located south of the Parkview Avenue (Open Space Access) to the Henderson Open Space and west of the southern portion of the project site between Henderson Road and the Cobblestone Shopping Center to the east. South of the 2.10-acre parcel are six residential dwelling units located on an approximate 0.47-acre parcel located west of the Henderson Road (North) and Parkview Avenue (South) intersection. These commercial use parcels are classified as "General Office" (GO) on the *General Plan* and zoned likewise.



Dignity Health North State Pavilion Project

Aerial Photograph

Figure 3-2



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Dignity Health North State Pavilion Project

USGS

Figure 3-3

The proposed project abuts the Cypress Avenue bridge to the north with office space and Lithia Motors to the northeast. To the east of Hartnell Avenue are offices and retail uses. To the southeast are two senior assisted living facilities and further south, the Department of Veterans Affairs Clinic. Refer to Table 3-1, EXISTING SURROUNDING LAND USES, Figure 3-4, EXISTING GENERAL PLAN CLASSIFICATIONS, and Figure 3-5, EXISTING ZONING DISTRICTS.

**Table 3-1
EXISTING SURROUNDING LAND USES**

Direction from Proposed Project Site	Existing Land Uses	Existing General Plan Classifications	Existing Zoning Districts
North	Cypress Avenue, Office, Retail	General Commercial (GC) Greenway (GWY)	General Commercial (GC) Open Space (OS)
East	Shopping Center, Hartnell Avenue	Shopping Center (SC)	Shopping Center (SC)
South	Auto Repair, Office Building, Two Undeveloped Parcels, Henderson Open Space	General Commercial (GC) General Office (GO) Greenway (GWY)	General Commercial (GC) General Office (GO) Open Space (OS)
West	Henderson Open Space, Single Family Home, Sacramento River	General Office (GO) Greenway (GWY)	General Office (GO) Open Space (OS)

Source: City of Redding. 2000-2020 General Plan. October 2000; City of Redding GIS. 2018.

North

To the north of the proposed project is Cypress Avenue and then an office building with open space to the west. On the east side of the Cypress Avenue and Hartnell Avenue intersection is Lithia Motors. The developed land is classified as “General Commercial” (GC) and the open space is “Greenway” (GWY).

East

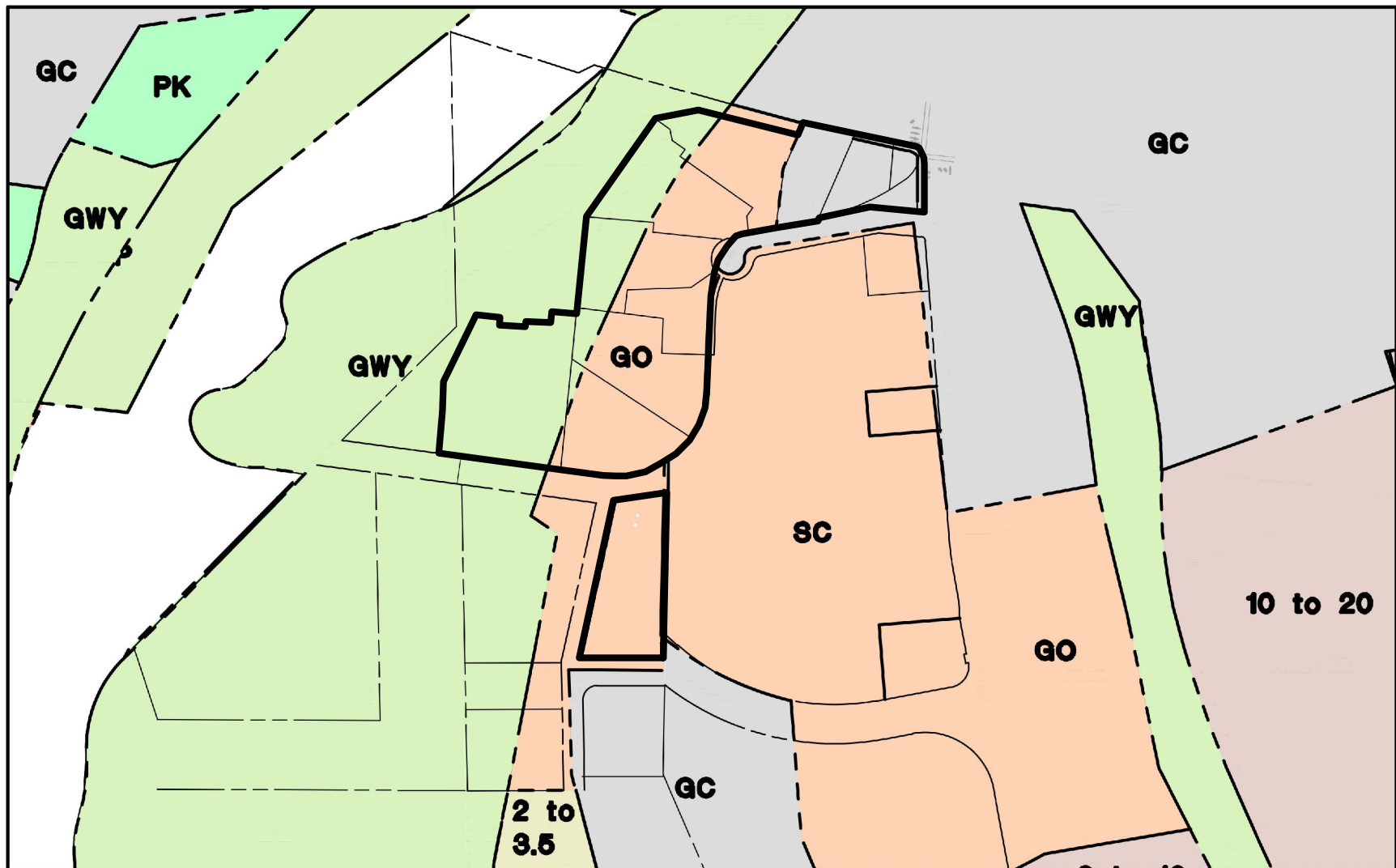
Immediately to the east of the proposed project site is the Cobblestone Shopping Center much of which is vacant. However, a new owner is remodeling and actively pursuing tenants. South of the main portion of the project site is a project area of approximately 1.14 acres between Henderson Road (North) to the west and the Cobblestone Shopping Center to the east. The parcel abuts Parkview Avenue to the south.

South

South of the main project area is Parkview Avenue that intersects with Henderson Road (South). West of Henderson Road is a vacant 2.09-acre parcel and then another vacant 4.50-acre parcel both of which are classified as “General Office” (GO) on the *General Plan* and zoned as such. Immediately west of the 4.50-acre parcel is the Henderson Open Space classified as “Greenway” (GWY) and zoned “Open Space” (OS). The 1.14 acres of the project that lies between Henderson Road and the Cobblestone Shopping Center abuts Parkview Avenue to the south. Beyond Parkview Avenue is a small vacant office building and an auto repair facility.

West

Land immediately west of the main portion of the proposed project is the Henderson Open Space that is designated “Greenway” (GWY) and zoned “Open Space” (OS) with the Sacramento River further west.



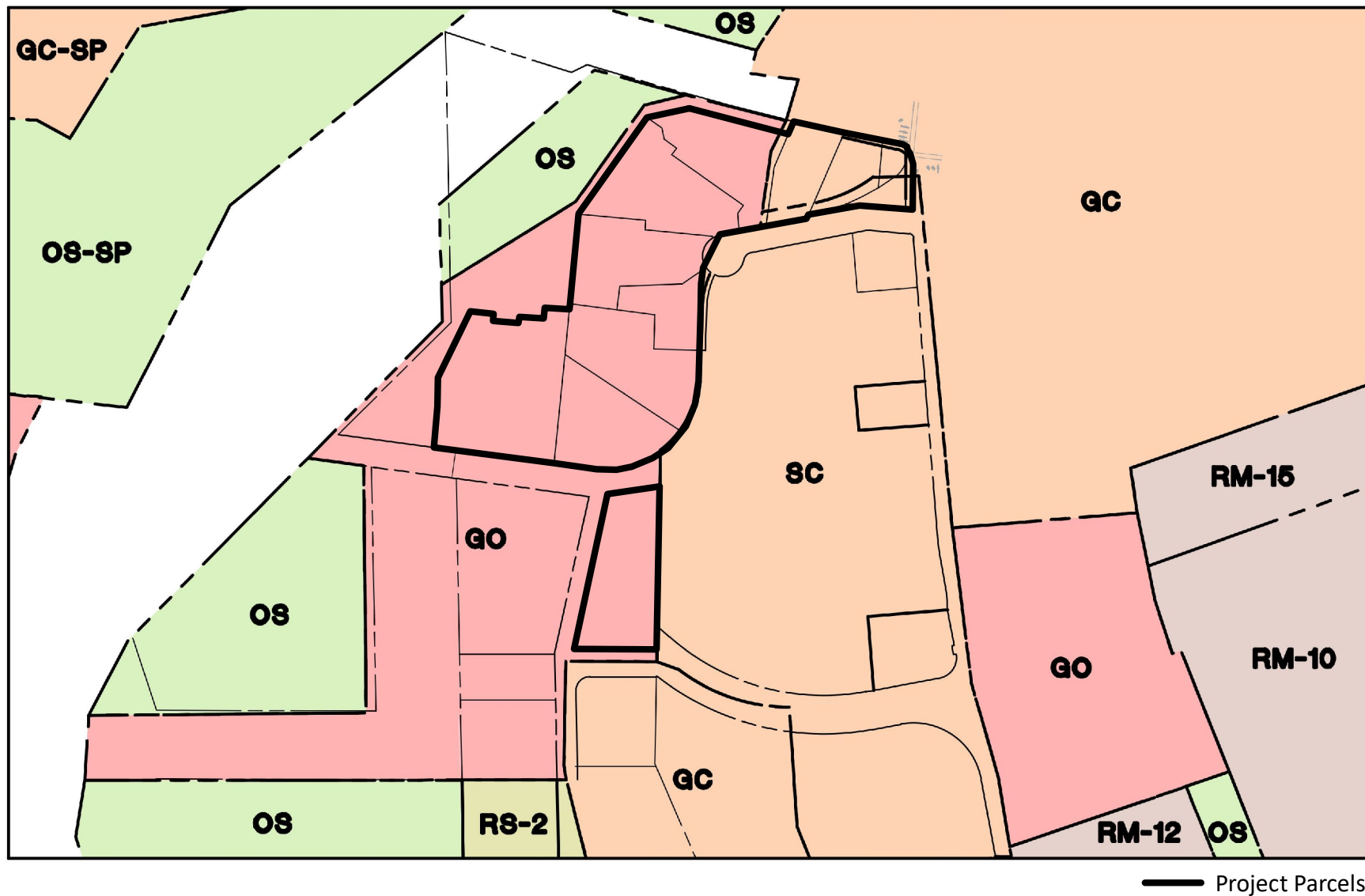
— Project Parcels



Dignity Health North State Pavilion Project

Existing General Plan Classifications

Figure 3-4



Dignity Health North State Pavilion Project

Existing Zoning Districts

Figure 3-5

3.2 BACKGROUND AND HISTORY

In the late 1940s, the Sisters of Mercy assumed responsibility for St. Caroline's Hospital, now known as Mercy Medical Center Redding located at 2175 Rosaline Avenue owned and operated by Dignity Health, the fifth largest health system in the nation with more than 40 hospitals across three states. For more than 70 years, Mercy Medical Center Redding has offered comprehensive health care services in the North State, providing acute care and a full-range of medical and surgical services. The 267-bed hospital is located on approximately 18.21 acres located 1.7 miles west of the proposed project. According to the City's *2016 Comprehensive Annual Financial Report*, Mercy Medical Center with 1,832 employees is the largest employer in the City.

Commencing in the summer of 2015, Dignity Health and the City of Redding, as the Successor Agency to the Redding Redevelopment Agency (Successor Agency), discussed the potential sale of City property owned within the proposed project site. On January 19, 2016, the City Council and the Successor Agency Board approved a Letter of Intent to sell City and Successor Agency properties to Dignity necessary for the proposed project. The Successor Agency parcels are identified as Assessor's Parcel Number (APN) 107-430-057, APN 107-430-034, and APN 107-400-008; the City's parcel is identified as APN 107-430-033. In addition, Dignity needed a 1.82-acre portion of another 2.81-acre parcel (APN 107-500-023) owned by the City for parking for the proposed project. The parcel split was to occur simultaneous with the close of escrow.

On August 16, 2016, the City Council and Successor Agency approved the sale the five parcels for \$641,000. Three of the parcels, with a combined sale price of \$530,000, belonged to Redding's former Redevelopment Agency, whose proceeds would be returned to the State of California. The City received approximately \$180,000 from the sale.

Under terms of the sale, Dignity agreed to award the City a \$21,000 community grant to pay half the cost of an environmentally friendly parking lot for users of a proposed public kayak and raft launch referenced as the Henderson Open Space. In addition, Dignity agreed to reimburse the City up to \$350,000 if the wellness center causes the City to lose \$350,000 in grants previously awarded by the California Division of Boating and Waterways for construction of the launch ramp. Dignity also agreed to contribute at least \$80,000 for improvements to the Henderson Open Space and provide room for the Dobrowsky House if a group comes forward to pay for the home's relocation. The large Craftsman-style home was located at the corner of Yuba and Oregon streets and was slated for demolition once the new Shasta County Courthouse project was funded. However, the Dobrowsky House was demolished so this term of the agreement is no longer applicable. Dignity Health, the Successor Agency, and the City would each be responsible for a portion of the closing costs. The Successor Agency and the City would each be required to pay a portion of a three percent commission to the real estate broker. The sale was approved by the Oversight Board on August 26, 2016.

On July 10, 2017, Dignity began clearing out debris, trash and the remains including foundations of old buildings at the project site. Before removing the remnants of the previous buildings, trash was removed from the area, which has been a popular camping spot for homeless. "Opening and clearing the space will not only prepare the area for the wellness center, but also make the area safer for the community and other businesses in the area, Dignity said in a news release." (Record Searchlight July 10, 2017.)

Dignity Health submitted applications for entitlements for the proposed development in January 2017. After a public hearing on August 22, 2017, the Planning Commission continued the meeting indefinitely to allow staff and Dignity time to review comments received in response to the CEQA Initial Study/Mitigated Negative Declaration prepared for the project. To address the comments, in early November 2017 Dignity announced their intention to work with the City to prepare an EIR.

LAND USE AND ENTITLEMENT HISTORY

The ensuing discussion is derived from the *Phase II Subsurface Archaeological Testing and Evaluation of Site CA-Sha-214 (P-45-000214) for the North State Pavilion Project, City of Redding, Shasta County, California*, prepared by Natural Investigations Company. The discussion provides a concise overview illustrating that the proposed project site and vicinity experienced intensive development over the years.

“The project is located within the territory historically occupied by Native Americans, principally the Wintu at the time of historic contact (1830-40). The Wintu inhabited the project vicinity primarily west of and along the Sacramento River, while the Central Yana inhabited the area about seven miles east of the Sacramento River, although salmon and other fish were caught by the Yana from the river.

Cattle were raised on the 160-acre Hempsted Ranch that was settled in 1886 and located approximately 1.4 miles northeast of the proposed project site at the present location of Dana Drive and Canby Road in Redding. An irrigation wheel was located on the Sacramento River southwest of the Project area. As shown on historic maps, a horse race track operated by Bassett and Touhey in 1890 was located near present-day Redding City Hall on Cypress Avenue west of the Sacramento River.

Review of historic aerial photographs and historic maps indicate the proposed project vicinity, including the project area remained undeveloped from 1894 to 1913, except for Free Bridge located directly west of present-day Parkview Avenue and built across the Sacramento River in the early 1990s. By 1944, the first Cypress Avenue Bridge had replaced Free Bridge, and residential and commercial development occurred in the project vicinity beginning in 1944. Free Bridge was subsequently dismantled in the mid-1950s

By 1944, residential buildings were located on the southern arm of the project area, although the rest remained undeveloped. From 1964 to 1967, Hein J H Co. sand and gravel occupied this southern arm. By 1969, the buildings were gone, and the land used as a storage yard. To the west of the southern arm, outside the project area, a lumber mill was present between 1944 and 1988, operated by Calaran Lumber Corporation from 1980 to 1988.

By 1952, a concrete plant had been established in the southern half of the project area. The facility was operated by Redding Transit Mix Inc. from 1952 to 1997 and also by Kettlewell J Rexford ready mix concrete in 1959. Other businesses also used the land owned by the concrete facility, including Don's Auto Repair from 1988 to 1997, Hard Rock Construction Inc. building contractors from 1985 to 1998, and Dan Palmer Trucking in 1989. An abandoned fuel dispensing station with an underground storage tank (UST) was located on the property in 1997, the concrete facility removed by 1998, and one UST removed in 2007. Most of the structures associated with the plant have been removed or partially demolished. The structural remains of the facility (concrete slab foundations, isolated pillars, and partial retaining walls and loading ramps), recorded as the Henderson Concrete Plant archaeological site, are in poor condition, and were found ineligible for listing in the National Record of Historic Places or California Register of Historic Resources.

In the extreme northeast corner of the project area, a gasoline service station (Gasamat Station) was in operation from 1972 to 1998; closed following removal of contaminated soils from a leaking UST. On the adjacent parcel to the west, a commercial building was present by 1969, greenhouses added in 1983 and removed by 1993, and the commercial building demolished in 2007. The commercial building was occupied by C&L Transmission Repair from 1964 to 2007. The masonry block structure had a concrete slab foundation. The business included a warehouse, five storage containers, machine shop building, and a septic system, but no USTs. The 1998 aerial shows the adjacent land to the west as having been disturbed by vehicles and as being used for what appears to be equipment storage, presumably by the transmission repair business.

Near the center of the project area, the two extant, conjoined buildings, which have a combined footprint of 23,000 square feet and a physical address of 2442 and 2456 Henderson Road, were constructed in 1980. The shopping centers bounding the Project area on the east (Cobblestone Shopping Centers) were also built in the early 1980s.

Between 1980 and 2015, there was a myriad series of tenants in the existing buildings in the project area. These included three office-related establishments (ABC Printers [1981–1992], Signs & Wonders Custom Lettering and Pinstriping [1981–1985], and Blueprint Shop [1994–1999]); Augie’s Enterprise Lawnmower Repair (2013–2015); and six automotive services during the 25-year period between 1985 and 2009: Custom Car Clean auto detailing (1985), Carburetors Unlimited (1985–1989), Trim Line Car Care auto detailing (1988–2002), Superior Automotive Service (1992), Redding Four Wheel Drive (1993–1995), and D&L Auto Repair shop (1999–2009).

By 2005, several dirt roads throughout the project area north of the extant buildings are visible on the historic aerials. By July 2007, there are multiple small buildings, trailers or containers and construction supplies on this area, which appears to have been leveled north of the parking area for the existing buildings. This delineates the staging area for the Cypress Bridge Replacement Project. The land in the northeast corner had been vacant since removal of the gasoline station in 1998 and the adjacent parcel to the west vacant since closure of C&L Transmission Repair in 2007. No specific history of building or enterprise development is evident for the remainder of the land in the project area between the existing buildings and the northeast section, but it appears to have been used by the transmission repair business for equipment storage. By 2010, as shown on aerial photographs, the amount of construction material and containers covered nearly the entire area. Used as a storage yard and staging area for the Cypress Bridge Replacement Project, all the material is gone based on the July 2011 aerial.”

Other than the proposed project application for entitlements, the most recent entitlements sought for a portion of the proposed project site, was a use permit (UP-39-92) to construct two restaurants with a total of 450 seats, 19,248 square feet of office and 5,568 square feet of retail on APNs 107-500-17, 18, 19 and 20 was originally approved on August 19, 1992. A use permit extension was approved on August 3, 1995. Subsequently, on August 27, 1997 a new use permit (UP-29-97) was approved to replace the expired UP-39-92 with the same proposed uses. The proposed project was not constructed.

It should be noted that a property line adjustment was completed on January 10, 2017 to increase the City’s open space area west of the proposed project and near the Cypress Avenue bridge, and to accommodate a trail from Cypress Avenue to the Henderson Open Space and the Sacramento River through the project site.

3.3 GENERAL PLAN AND ZONING

The City of Redding's *General Plan* was adopted in October 2000 and serves as the overall guiding policy document for land use, development, and environmental quality in the City of Redding for the next 20 years. The *General Plan* includes policies, standards, implementation programs, quantified objectives (four housing), the General Plan Diagram, and circulation diagrams. The *General Plan* planning area is divided into five primary sectors, each of which is shaped by its unique characteristic, history, and issues. The proposed project is located in the South Redding Planning Area. The existing *General Plan* classifications and zoning district designations are summarized in Table 3-2, PROJECT SITE INFORMATION, and illustrated on Figure 3-4 and Figure 3-5.

**Table 3-2
PROJECT SITE INFORMATION**

APN	Gross Acreage	Existing Land Use	Existing General Plan Classifications	Existing Zoning District Designations
107-400-008	1.14	Vacant & Outside Storage	General Office (GO)	General Office (GO)
107-400-009	0.25	Right-of-Way	General Office (GO) & General Commercial (GC)	General Office (GO) & General Commercial (GC)
107-430-033	0.02	Vacant Remnant Right-of-Way	General Commercial (GC)	General Commercial (GC)
107-430-034	0.13	Vacant	General Commercial (GC)	General Commercial (GC)
107-430-057	0.43	Vacant	General Commercial (GC)	General Commercial (GC)
107-430-059	0.49	Vacant with Billboard	General Commercial (GC)	General Commercial (GC)
107-500-017	0.48	Two Conjoined Buildings	General Commercial (GC)	General Office (GO)
107-500-018	0.95	Vacant	General Office (GO) & Greenway (GWY)	General Office (GO)
107-500-019	1.07	Vacant	General Office (GO)	General Office (GO)
107-500-020	0.90	Vacant	General Commercial (GC) & General Office (GO)	General Office (GO)
107-500-021	0.09	Right-of-Way	General Commercial (GC)	General Commercial (GC)
107-500-024	1.07	Vacant	General Office (GO) & Greenway (GWY)	General Office (GO)
107-500-025	0.95	Vacant	General Office (GO) & Greenway (GWY)	General Office (GO)
107-500-028	1.82	Vacant	General Office (GO) & Greenway (GWY)	General Office (GO)
Misc. ROW	0.76	Right-of-Way	General Office (GO) & General Commercial (GC)	General Office (GO) & General Commercial (GC)
Total Project Area	10.55			

Source: City of Redding. 2018; ENPLAN Parcel Viewer. 2018.

EXISTING GENERAL PLAN CLASSIFICATIONS

The *General Plan* classifies the proposed project site as "General Office" (GO), "General Commercial" (GC) and "Greenway" (GWY). The following provides a brief description of the land use classifications:

General Office (GO)

This classification provides sites for professional and business offices and personal-service businesses with appropriate and compatible accessory uses. This classification may also be used for rest homes; nursing homes; day-care facilities; hospitals; religious, educational, cultural, and public-utility uses; multiple-family housing; and financial institutions. Ancillary retail uses, such as pharmacies, are appropriate as are "sit-down" restaurants.

General Commercial (GC)

This classification is intended for existing commercial strips on arterial streets and interchange areas where commercial concentrations are desirable. Allowable uses include retail stores, offices, and personal-service establishments. Financial institutions, private schools, day-care facilities, convalescent hospitals, automobile sales and service, and similar uses also may be permitted subject to appropriate standards. Scale of development, including individual buildings, will be at a smaller scale than that found in developments servicing a regional market.

Greenway (GWY)

"Greenway" (GWY) is natural open space and includes slopes in excess of 20 percent and the 100-year floodplain of the Sacramento River and its tributary creeks and streams. Natural open space is located predominantly along the Sacramento River, its tributary streams, and the bluffs and creeks of West and East Redding. Because of the inherent dangers to life and property and irrevocable damage to the natural environment, these natural land and water areas should not be urbanized or significantly altered. The specific limits of "Greenway" (GWY) on any property must be determined by a topographic or flooding analysis particular to that property. These natural areas are an important local resource and serve as places in which flora or fauna can be maintained in its natural state. In addition, these areas provide relief from urbanization, reduce siltation from excessive grading, buffer various land use activities and transportation routes, are an important visual resource, and can be part of the City's urban trail system.

EXISTING ZONING DISTRICT DESIGNATIONS

Redding Municipal Code (RMC) Title 18 contains the City's *Zoning Ordinance*. The broad purposes of the *Zoning Ordinance* are to implement the *General Plan* and promote and protect the public health, safety, peace, comfort, and general welfare of the City of Redding. Additionally, development standards are established for each district to ensure that activities can be reasonably accommodated in a manner that is compatible with adjacent land uses. The City's *Zoning Ordinance* is intended to:

- *Direct growth with a priority on those areas where infrastructure and urban services can be economically provided.*
- *Ensure consistency between General Plan designations and policies and zoning districts.*
- *Provide compatibility between land uses.*
- *Establish standards regulating the use and physical development of land.*

Currently the site's zoning district designations are the proposed project site as "General Office" (GO) and "General Commercial" (GC). The following provides a brief description of site's existing zoning district designations:

General Office (GO)

The specific purposes of the "office" districts are to:

- *Provide appropriately located areas for a full range of office uses needed by the city's businesses and workers;*
- *Minimize the impact of commercial development on adjacent residential districts.*

Additionally, the purpose of the “General Office” (GO) district is to *“provide sites for professional, business, and personal-service businesses with appropriate and compatible accessory uses. Provide for other compatible uses including: rest homes, nursing homes, day-care facilities, hospitals; religious, educational, cultural and public utility uses; and financial institutions. Ancillary retail uses, such as pharmacy and sit-down restaurants, are also appropriate.”*

General Commercial (GC)

The specific purposes of the "General Commercial" (GC) districts are to:

- *Provide for a full range of commercial uses;*
- *Strengthen the city's economic base and provide employment opportunities for residents of the city;*
- *Identify appropriate standards for reviewing proposals for new development and redevelopment, where appropriate in commercial areas;*
- *Ensure the provision of services and facilities needed to accommodate planned population densities;*
- *Encourage the creation of neighborhood activity centers as focal points along transportation corridors;*
- *Define the types of uses that are appropriate in each zoning district and, if applicable, zoning subdistrict.*

Additionally, the purpose of the “General Commercial” (GC) district is to *“maintain areas on arterial streets, near interchanges, and in existing commercial strips for commercial uses. Certain general commercial areas are identified for "visitor" and "retail" uses on the zoning map to assist in achieving the policies of the General Plan.”*

3.4 PROJECT CHARACTERISTICS

Dignity Health’s Mercy Medical Center of Redding (herein referred to as the project applicant) is proposing the development of the North State Pavilion Project in a campus-like setting whereby, the buildings are compatible with each other from a site planning and architectural design perspective. The project is a wellness center for ambulatory medical offices and clinics distributed amongst three buildings totaling approximately 129,600 square feet with associated parking, landscaping and infrastructure on 10.55 acres of land.

As previously discussed above, State *CEQA Guidelines* require that an EIR provide “[a] general description of the project’s technical, economic, and environmental characteristics, considering the principal engineering proposals, if any, and supporting public service facilities.” (State *CEQA Guidelines* §15142[c].)

The proposed project affects all, or portions of 14 assessor parcels as shown on Table 3-3, NORTH STATE PAVILION ASSESSOR’S PARCELS. Three of the parcels are existing street rights-of-way (ROW). The Henderson Road (North), Henderson Road (South), and Parkview Avenue (Open Space Access) ROWs will be subject to improvements.

Table 3-3
NORTH STATE PAVILION ASSESSOR'S PARCELS

Assessor's Parcel Number	Acreage	Status	Ownership
107-400-008	1.14	Vacant & Outside Storage	Successor Agency
107-400-009	0.25	ROW	City of Redding
107-430-033	0.02	Vacant	Dignity Health
107-430-034	0.13	Vacant	Successor Agency
107-430-057	0.43	Vacant	Successor Agency
107-430-059	0.49	Vacant with Billboard	Dignity Health
107-500-017	0.48	Two Conjoined Buildings	Dignity Health
107-500-018	0.95	Partial Building	Dignity Health
107-500-019	1.07	Vacant	Dignity Health
107-500-020	0.90	Vacant	Dignity Health
107-500-021	0.09	ROW	City of Redding
107-500-024	1.07	Vacant	Dignity Health
107-500-025	0.95	Vacant	Dignity Health
107-500-028	1.82	Vacant	Dignity Health
Total	9.79		

Note: Acreages are approximate.

PROPOSED PROJECT ENTITLEMENTS

Required entitlements for the development of the proposed project include a general plan amendment, rezoning, use permit, and a parcel map. The following discusses each entitlement being requested:

General Plan Amendment

Amend the *General Plan* from the existing land use classification of "General Office" (GO), "General Commercial" (GC), and "Greenway" (GWY) to "Public Facilities" (PF-I). Table 3-4, EXISTING AND PROPOSED GENERAL PLAN & ZONING, identifies the existing *General Plan* land use classifications, acreages, proposed classifications and resultant acreages due to property line adjustments.

Table 3-4
EXISTING AND PROPOSED GENERAL PLAN & ZONING

Assessors' Parcel Number	Existing General Plan Classifications (acreage)			Existing Zoning Districts (acreage)		Proposed General Plan / Proposed Zoning (acreage)
	GO	GC	GWY	GO	GC	PF-I/PF
107-400-008	1.14	-	-	1.14	-	1.14
107-400-009*	0.115	0.115	-	0.115	0.115	0.25
107-430-033*	-	0.02	-	-	0.02	0.02
107-430-034	-	0.13	-	-	0.13	0.13
107-430-057	-	0.43	-	-	0.43	0.43
107-430-059	-	0.49	-	-	0.49	0.49
107-500-017	0.48	-	-	0.48	-	0.48
107-500-018	0.49	-	0.46	0.95	-	0.95
107-500-019	0.53	-	0.54	1.07	-	1.07
107-500-020	0.73	-	0.17	0.90	-	0.90
107-500-021*	0.09	-	-	0.09	-	0.09
107-500-024	0.94	-	0.13	1.07	-	1.07
107-500-025	0.91	-	0.04	0.95	-	0.95
107-500-028	1.82	-	-	1.82	-	1.82
Misc. ROW	0.38	0.38	-	0.38	0.38	0.76
Totals	7.625	1.565	1.34	7.625	1.565	10.55

Note:

- *Denotes parcels that are City of Redding ROW. All acreages are approximate.

The City's Community Development and Design Element of the *General Plan* identifies that the "Public Facilities" (PF-I) land use "*classification is intended for public and quasi-public facilities, including, but not limited to, government services and facilities, fire stations, wastewater treatment facilities, electrical substations, airports, domestic water treatment and storage, landfills, and similar uses. It is also appropriate for institutional uses, such as schools and accredited secondary educational facilities, hospitals, and cemeteries, as well as appropriate lands controlled by philanthropic and nonprofit organizations for existing or future public uses. The General Plan Diagram may not depict public facilities sites that occupy less than two acres.*"

Rezoning

The existing zoning district designations are proposed to be rezoned from "General Office" (GO) and "General Commercial" (GC) to "Public Facilities" (PF). Table 3-4, above, identifies the existing and proposed zoning district designations and associated acreages by current APN. However, the parcel map will reduce the number of parcels to four. Chapter 18.36 of the RMC states that the specific purposes of the "Public Facilities" (PF) district are to:

- *Provide areas for public and quasi-public facilities that are appropriate for educational facilities, cultural and institutional uses, health services, specialized housing and care facilities, parks and recreation, general government operations, airports, utility and public service needs, and other similar and related supporting uses.*
- *Recognize the unique nature of public and quasi-public uses by requiring site development permits or use permits for most uses and activities within the "public facilities" district.*
- *Ensure the provision of services and facilities needed to accommodate planned population densities."*

Use Permit

A use permit request will allow for the development of the proposed project and to allow a portion of the parking lot to encroach into the Federal Emergency Management Agency (FEMA) regulated 100-year floodplain of the Sacramento River. Table 3-5, PROPOSED PROJECT ACREAGE, identifies all or portions of resultant assessor parcels, after property line adjustments, which comprise the proposed project acreage.

**Table 3-5
PROPOSED PROJECT ACREAGE**

APN	Acreage	APN	Acreage
107-400-008	1.14	107-500-019	1.07
107-400-009	0.25	107-500-020	0.90
107-430-033	0.02	107-500-021	0.09
107-430-034	0.13	107-500-024	1.07
107-430-057	0.43	107-500-025	0.95
107-430-059	0.49	107-500-028*	1.82
107-500-017	0.48	Misc. ROW**	0.76
107-500-018	0.95	Total Site Acreage	10.55

Notes:

1. * This new parcel is the result of a recent parcel split of APN 107-500-026.
2. ** Approximately 0.23 acres of the Parkview (South) ROW will include project related improvements including but not limited to, parking, sidewalks, driveways, and landscaping.

Parcel Map

A parcel map is necessary to combine the various parcels to create four separate parcels for the proposed project. Each building will be located on its own parcel and a fourth parcel will be held in common by each building parcel. The common area includes, but is not limited to; the parking areas, driveways, light fixtures, pole and monument signs, solid waste and generator enclosures, and the landscape and hardscape areas. Each building parcel will be entitled to a pro-rata share of the total parking necessary for each individual building. Reciprocal parking will be identified, as necessary.

PROPOSED ACTIVITIES AND OPERATIONS

As previously noted, the project applicant proposes the construction of three buildings totaling approximately 129,600 square feet for a wellness and prevention center in a campus like setting on the 10.55-acre project site (refer to Figure 3-6a, PROPOSED SITE PLAN, for an illustration of the proposed campus layout and Figures 3-6b, ENLARGED SITE PLAN – BUILDINGS ‘A’ & ‘C’, and Figure 3-6c, ENLARGED SITE PLAN – BUILDING ‘B’, that provide additional detail, including street cross-sections. In general, services are anticipated to be provided from 6:00 AM to 10:00 PM, Monday through Sunday.

In order to obtain a better understanding of the type of project being proposed by the project applicant, the following provides a general discussion and description regarding the concept of “wellness centers.

“A wellness center is a place where medical and mental health care professionals, life coaches and nutritionists provide services for a healthy mind and body. It is the promotion of healthy living, prevention of diseases and illnesses as well as treating certain conditions.”¹

An article published in Healthcare Finance discusses how “wellness systems improve overall health, in particular.”² The following are excerpts from the article:

“Hospital-based wellness centers, once seen as marketing tools for innovative health systems, have become a major part of a hospital's population health management program in the era of quality over quantity of care.”

‘The wave of the future is to focus less on illness and more on prevention and wellness,’ said Joan Phillips, vice president of Clinical Services at Beaumont Health in Troy, Michigan. ‘Our community and our employees want and need a place to focus on both fitness and wellness.’

Wellness centers attached to hospitals have evolved far beyond the local gym toward a medically integrated model. Most use exercise physiologists and other ancillary medical specialties offering a broader range of services. This also lets them set specific goals for their customers, holding them accountable for their own health.

¹ Moonstone Wellness Center. *What Is A Wellness Center?* [Online]: <http://moonstonewellnesscenter.com/what-is-a-wellness-center/>. Accessed: June 14, 2018.

² Healthcare Finance. August 17, 2015. *Wellness Centers, No Longer Hospital Gimmicks, Become Money-making Population Health Engines.* [Online]: <http://www.healthcarefinancenews.com/news/wellness-centers-no-longer-hospital-gimmicks-become-money-making-population-health-engines>. Accessed: June 14, 2018.



LEGEND

CITY PROJECTS (HENDERSON OPEN SPACE)

DECORATIVE FENCE

NOTE:

1. NO PARKING SIGNS SHALL BE INSTALLED ALONG MAIN ACCESS ROADWAYS, AS REQUIRED BY THE CITY OF REDDING.

REGISTERED PROFESSIONAL ENGINEER
 JAMES RUSSELL
 3182
 CIVIL
 OF CALIFORNIA

NOT FOR CONSTRUCTION

60' 0 60'
 (IN FEET)
 1 inch = 60 ft.

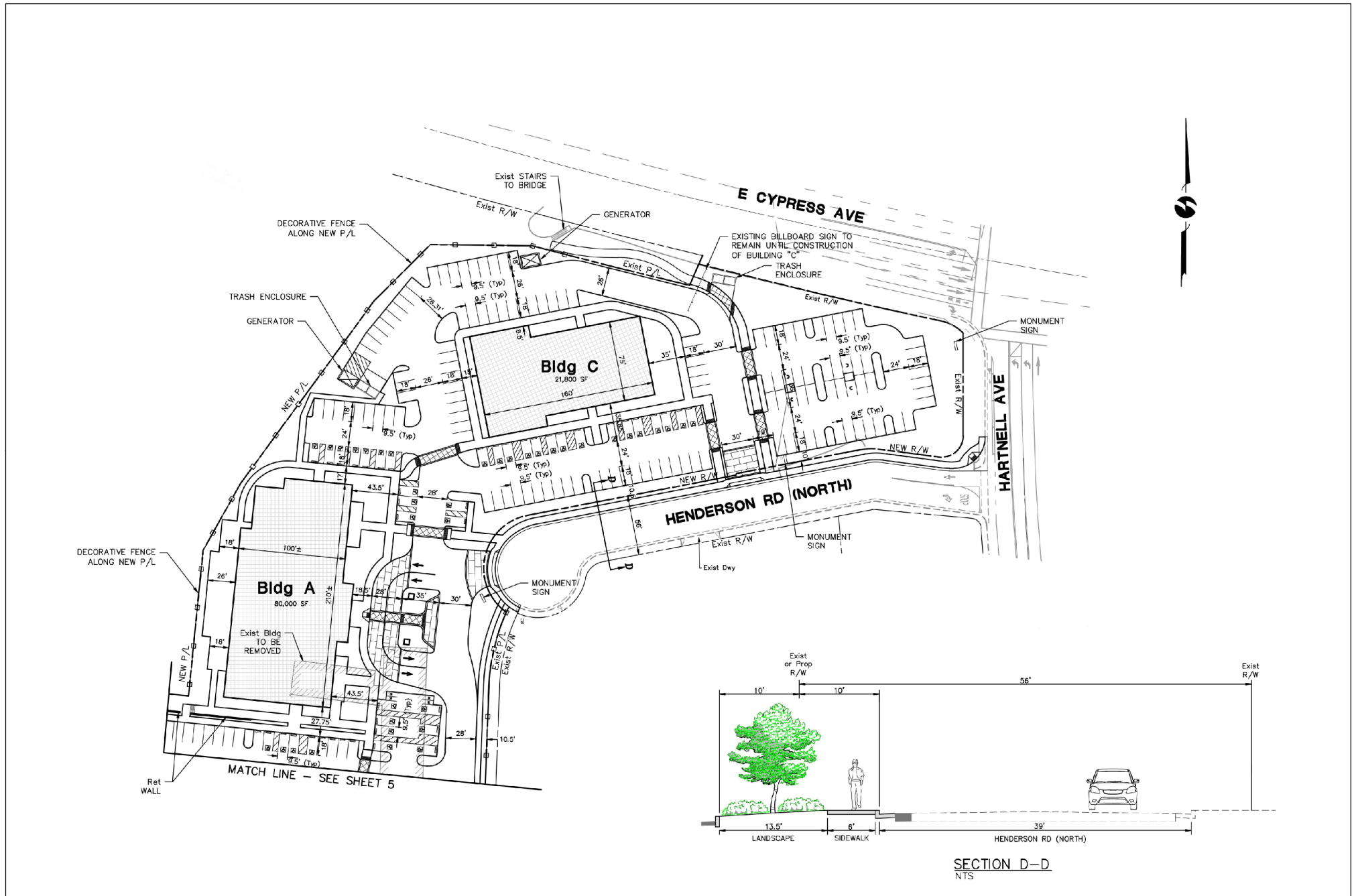
October 19, 2018



Dignity Health North State Pavilion Project

Proposed Site Plan

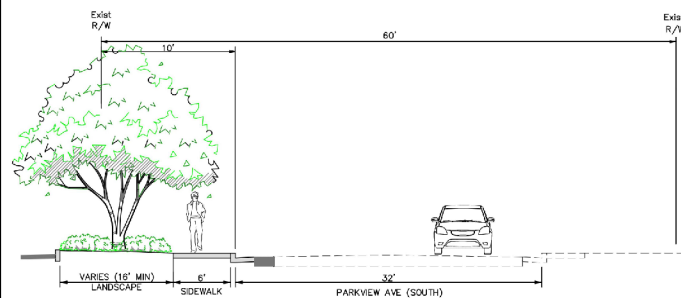
Figure 3-6a



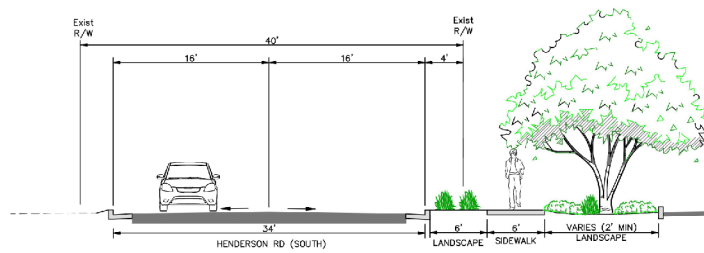
Dignity Health North State Pavilion Project

Enlarged Site Plan - Buildings 'A' & 'C'

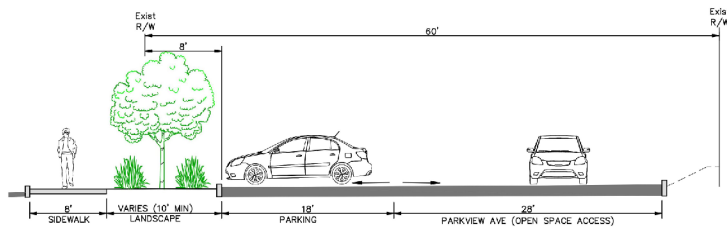
Figure 3-6b



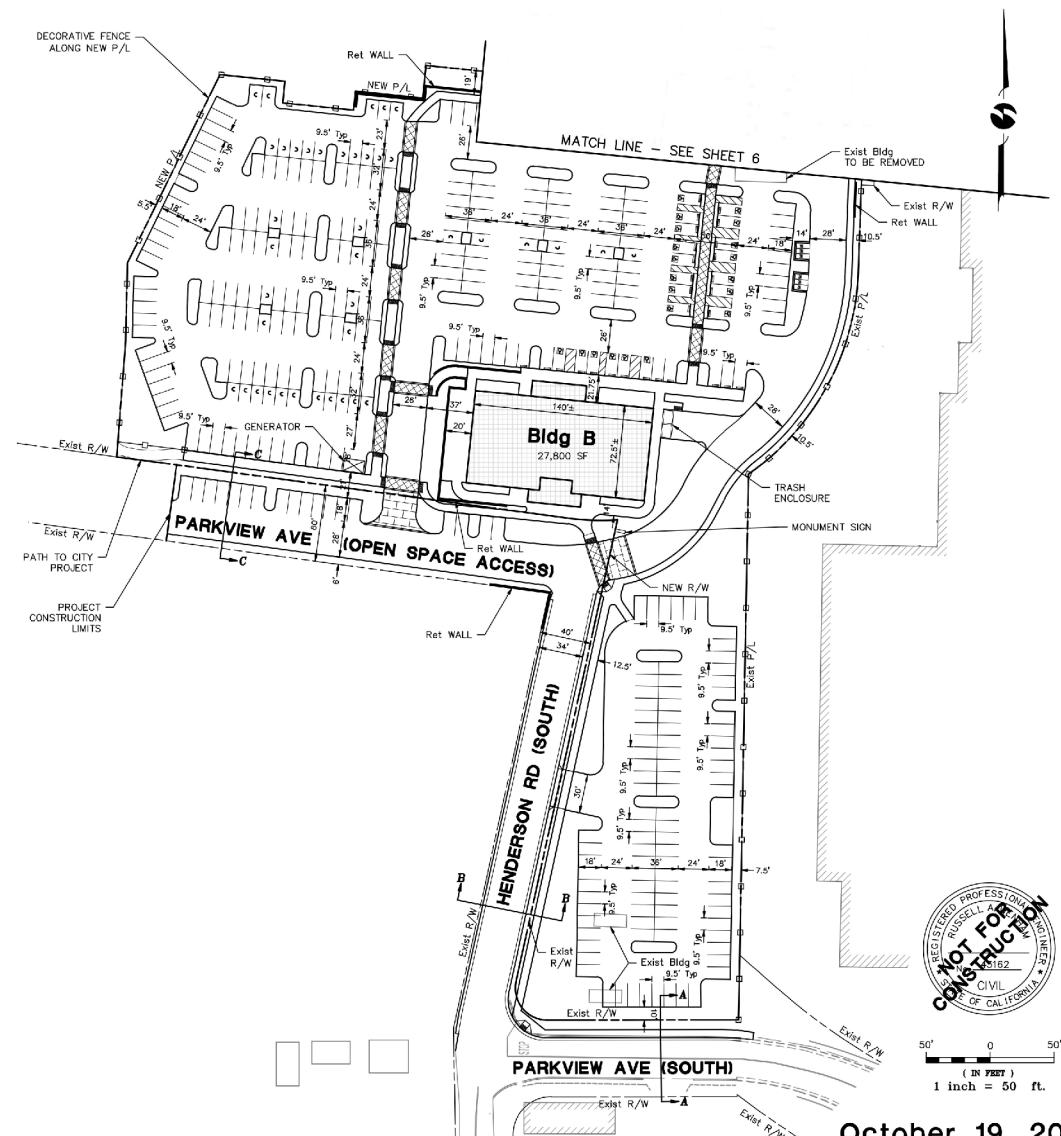
SECTION A-A
NTS



SECTION B-B
NTS



SECTION C-C
NTS



October 19, 2018



Dignity Health North State Pavilion Project

Enlarged Site Plan - Building 'B'

Figure 3-6c

‘Hospitals that understand the future are beginning to talk about the wellness center being the new front door to their health system,’ said Robert D. Boone, president and chief executive officer at the Medical Fitness Association in Richmond, Virginia. ‘Hospitals and doctors are being told they have the responsibility to manage their patients’ lifestyles. They obviously can’t do it in a couple of 15-minute visits every year, so this is where the medically integrated fitness centers come in.’”

The project applicant has several wellness and prevention centers throughout the three states they are located (California, Nevada and Arizona) where emphasis is placed on health risk assessments, screenings, exercise and wellness classes. Dignity’s St. Mary Medical Center Bazzani Wellness Center in Long Beach, California was established “for older adults to socialize, participate in educational programs, receive human services and enjoy recreational activities. Designed to meet the special needs of the city’s growing senior population, age 50 and older, the Wellness Center strives to provide adults with the tools to manage and maintain optimum health.” Services include but are not limited to lectures and seminars with physicians, exercise classes, health living workshops, smart-Driver course, health screenings, a resource library, free transportation (if needed) and a travel program.

Potential uses and services to be located within the proposed buildings may include, but are not limited to the following: Administrative Offices; Auditorium / Conference Rooms / Class Rooms; Cafeteria; Diagnostic Imaging; Electrical / Mechanical Rooms; Employee Lounge / Locker Rooms; Family Medicine / Pediatrics; Gift Shop; Janitorial Rooms; Laboratories; Orthopedics; Palliative Care; Pharmacy; Physical Therapy; Physician Offices; Radiology; Reception/Waiting Areas; Rehabilitation; Urgent Care Center; Visitor Lounges; and Women’s Health & Wellness. Cafeteria services, physical therapy and pharmacy services may more than likely be leased to outside service providers.

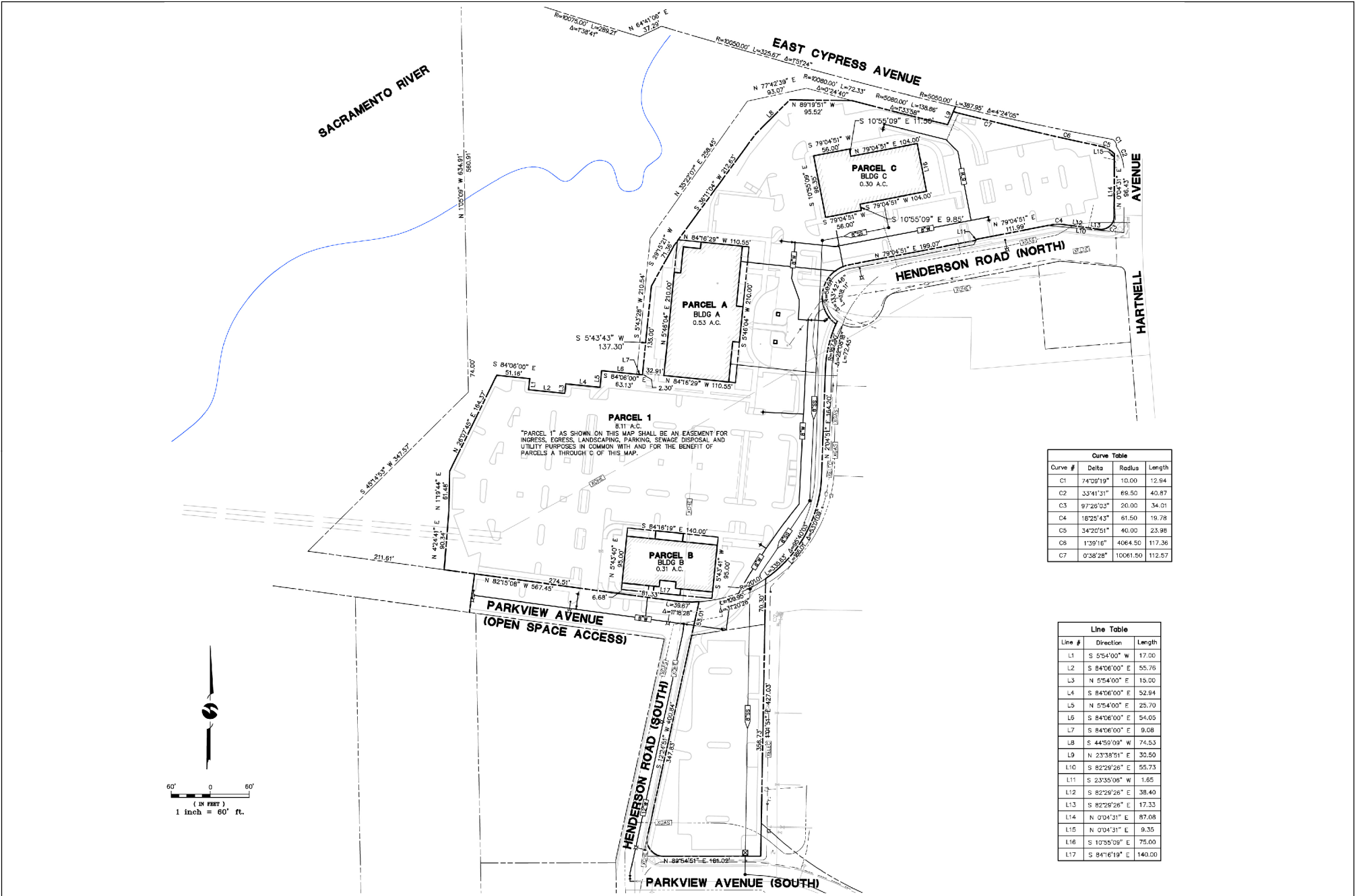
TECHNICAL CHARACTERISTICS

Development Summary

Development approval will allow the project applicant to construct three buildings that will total 129,600 square feet on the approximate 10.55-acre project site. As previously noted, three parcels will be created for each building and a fourth parcel will be held in common by each building parcel. Figure 3-7, TENTATIVE PARCEL MAP, identifies the various parcels. Parcel ‘A’ is 0.53 acres and contains Building ‘A’, Parcel ‘B’ is 0.31 acres for Building ‘B’, Parcel ‘C’ has 0.30 acres for Building ‘C’, and all the common features are located within Parcel ‘1.’ As noted on Figure 3-7, Parcel ‘1’ “shall be an easement for ingress, egress, landscaping, parking, sewage disposal and utility purposed in common with and for the benefit of Parcels ‘A’ through ‘C.’”

The proposed campus buildings will be sited for ease of access to all three buildings and associated parking from Henderson Road (North) with direct access from Hartnell Avenue, or from Henderson Road (South) with direct access to Parkview Avenue (South) which intersects with Hartnell Avenue. Building ‘A’ will have direct views to the Henderson Open Space to the west. A rendering of the proposed campus layout is illustrated in Figure 3-8a, PERSPECTIVE RENDERING. The proposed building elevations are shown in Figures 3-8b through 3-8d, ELEVATIONS, for Buildings ‘A’, ‘B’, and ‘C’, respectively.

Other project features include but are not limited to landscaping, hardscape features, three emergency generator enclosures in close proximity to each building, three solid waste bin enclosures near each building, decorative fencing, four monument signs, a pole sign, building signage, directional signing and parking lot, driveway and walkway lighting.



The exterior dimensions of the generator and trash enclosures are 11 by 18 feet and 8 by 22 feet, respectively. It is anticipated that the pole sign will be located near the intersection of Cypress Avenue and Hartnell Avenue. The height of the pole sign is still to be determined but is expected to comply with the RMC Chapter 18.42, *Signs*. The existing bill board adjacent to the Cypress Avenue bridge is not proposed to be removed until the construction of Building 'C' in Phase 2.

The project property line for which a property line adjustment was approved, between the project site and the Henderson Open Space will be delineated by a decorative metal fence ranging in height between four and six feet, as depicted on Figure 3-6b and Figure 3-6c. The decorative fence will also run along the eastern property line between Parkview Avenue (South) and Henderson Road (North).

Table 3-6, SITE DEVELOPMENT SUMMARY, provides details on the specific buildings and other site development features including, but not limited to landscape areas, parking spaces by type and areas, estimates of the various proposed onsite and offsite project ROW related improvements.

Table 3-6
SITE DEVELOPMENT SUMMARY

BUILDING AREA & MAXIMUM HEIGHTS	
Building 'A' – Four Stories – Varies 64 Feet to 72 Feet	80,000 Square Feet
Building 'B' – Three Stories – Varies 52 Feet to 58 Feet	27,800 Square Feet
Building 'C' – Two Stories – Varies 36 Feet to 44 Feet	27,800 Square Feet
	<i>Total Area – 129,600 Square Feet</i>
SITE COVERAGE	
Acres (Percent Site Coverage)	
City Roads & Right-of-Way	1.32 Acres (12.51%)
Primary Driveway & Sidewalk	0.40 Acres (3.79%)
Parking & Circulation	5.72 Acres (54.22%)
Building Footprint	1.00 Acres (8.72%)
Landscape – Parking Area	0.97 Acres (9.19%)
Landscape – Site & Building Area	1.14 Acres (10.80%)
	<i>Total Area – 10.55 Acres</i>
PARKING COUNT BY TYPE	
Standard Parking	425 (Includes 18 Parkview Avenue On-Street Parking)
Compact Parking – Maximum Allowable 55 Spaces	44
Motorcycle Parking – Maximum Allowable 16 Spaces	10
Standard Accessible Parking	59 (55 Required)
Van Accessible Parking	11 (7 Required)
	<i>Total Parking - 549 (548 required by City code)</i>
LANDSCAPE AREAS	
Landscape Based on Building Gross Floor Area	49,720 Square Feet (38.36% of Building Gross Floor Area)
Parking Area Landscape	42,380 Square Feet (32,940 Square Feet Required)
Parking Trees	142 (138 Required)
	<i>Total Area – 92,100 Square Feet</i>

Source: GHD; Title Sheet 1; Landscape Layout Sheet 17; June 9, 2017. NMR Elevations Sheets 28-30; June 8, 2017.

The project is currently proposed to develop in two phases. Phase 1 includes Building 'A' comprised of 80,000 square feet which is projected to be completed in 2022. Buildings 'B' and 'C' are considered Phase 2 construction that is expected to begin in 2024. It is estimated that up to 180 persons will be employed once the proposed project is completed. Figure 3-9, PROJECT PHASING PLAN, illustrates the areas to be developed with each phase and also provides a table that identifies a parking summary for each phase. It is anticipated that the staging area for both Phase 1 and 2 will be the parcel at the south end of the project, located north of Parkview Avenue (South) and east of Henderson Road (South).



VIEW A
FROM ACROSS RIVER, BELOW BRIDGE



Dignity Health North State Pavilion Project

Perspective Rendering

Figure 3-8a



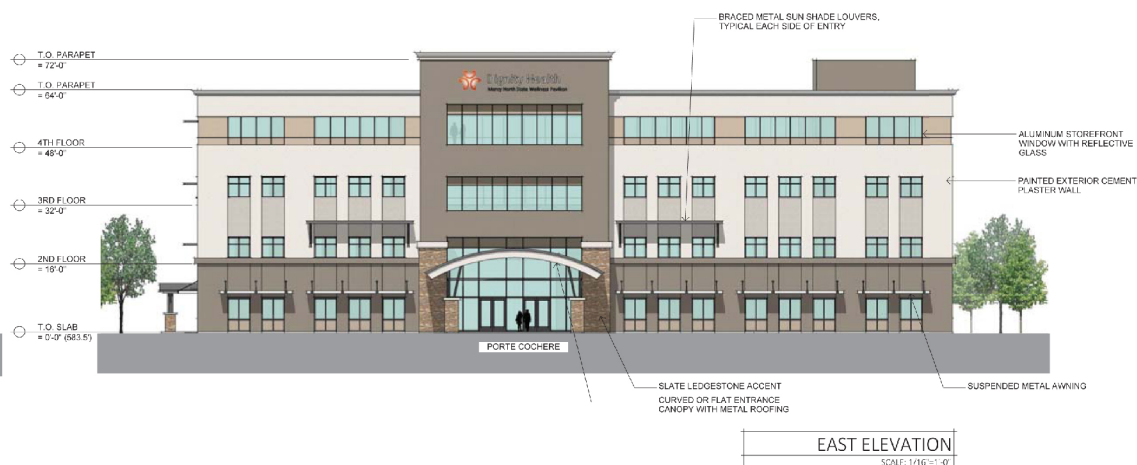
NORTH ELEVATION
SCALE: 1/16"=1'-0"



WEST ELEVATION
SCALE: 1/16"=1'-0"



SOUTH ELEVATION
SCALE: 1/16"=1'-0"



EAST ELEVATION
SCALE: 1/16"=1'-0"

BUILDING 'A' ELEVATIONS



Dignity Health North State Pavilion Project

Elevations - Building 'A'

Figure 3-8b



EAST ELEVATION
SCALE: 1/16"=1'-0"



NORTH ELEVATION
SCALE: 1/16"=1'-0"



SUSPENDED METAL AWNING AT SIDE ENTRANCE

WEST ELEVATION
SCALE: 1/16"=1'-0"

BUILDING 'B'



T.O. PARAPET
= 58'-0"
T.O. PARAPET
= 62'-0"
3RD FLOOR
= 32'-0"
2ND FLOOR
= 16'-0"
T.O. SLAB
= 0'-0" (5/8" S)

SUSPENDED METAL AWNING

SOUTH ELEVATION
SCALE: 1/16"=1'-0"

ALUMINUM STOREFRONT
WINDOW WITH REFLECTIVE
GLASS
PAINTED TILT-UP CONCRETE
OR PAINTED EXTERIOR
CEMENT PLASTER WALL

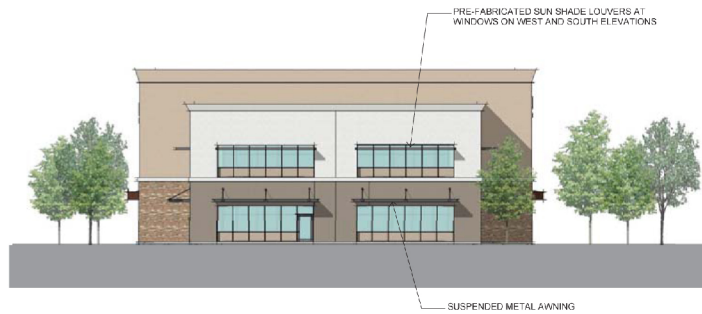
SLATE LEDGESTONE ACCENT
CURVED OR FLAT ENTRANCE CANOPY WITH METAL ROOF



Dignity Health North State Pavilion Project

Elevations - Building 'B'

Figure 3-8c



EAST ELEVATION
SCALE: 1/16"=1'-0"



NORTH ELEVATION
SCALE: 1/16"=1'-0"



WEST ELEVATION
SCALE: 1/16"=1'-0"



SOUTH ELEVATION
SCALE: 1/16"=1'-0"

BUILDING 'C' ELEVATIONS



Dignity Health North State Pavilion Project

Elevation - Building 'C'

Figure 3-8d

The project proposes ROW improvements to Henderson Road (North), Parkview Avenue (South), Henderson Road (South), and Parkview Avenue (Open Space Access). The improvements include, where applicable; street widening, paving and repaving, lane striping, curb, gutters, sidewalks, and drainage structures. All utilities, including water, sewer, stormwater, electrical, natural gas, cable and telephone service lines and conduits will be undergrounded. Fire hydrants and electrical transformers will be located per City of Redding standards and requirements.

Architecture

As previously identified above, Figure 3-8a shows the location of the three buildings interspersed on the site and visually “tied together” in a campus like setting with landscaping, both adjacent to the buildings and within the parking areas, that provide visual corridors primarily of the existing riparian areas within the Henderson Open Space and beyond to the Sacramento River to the west and northwest.

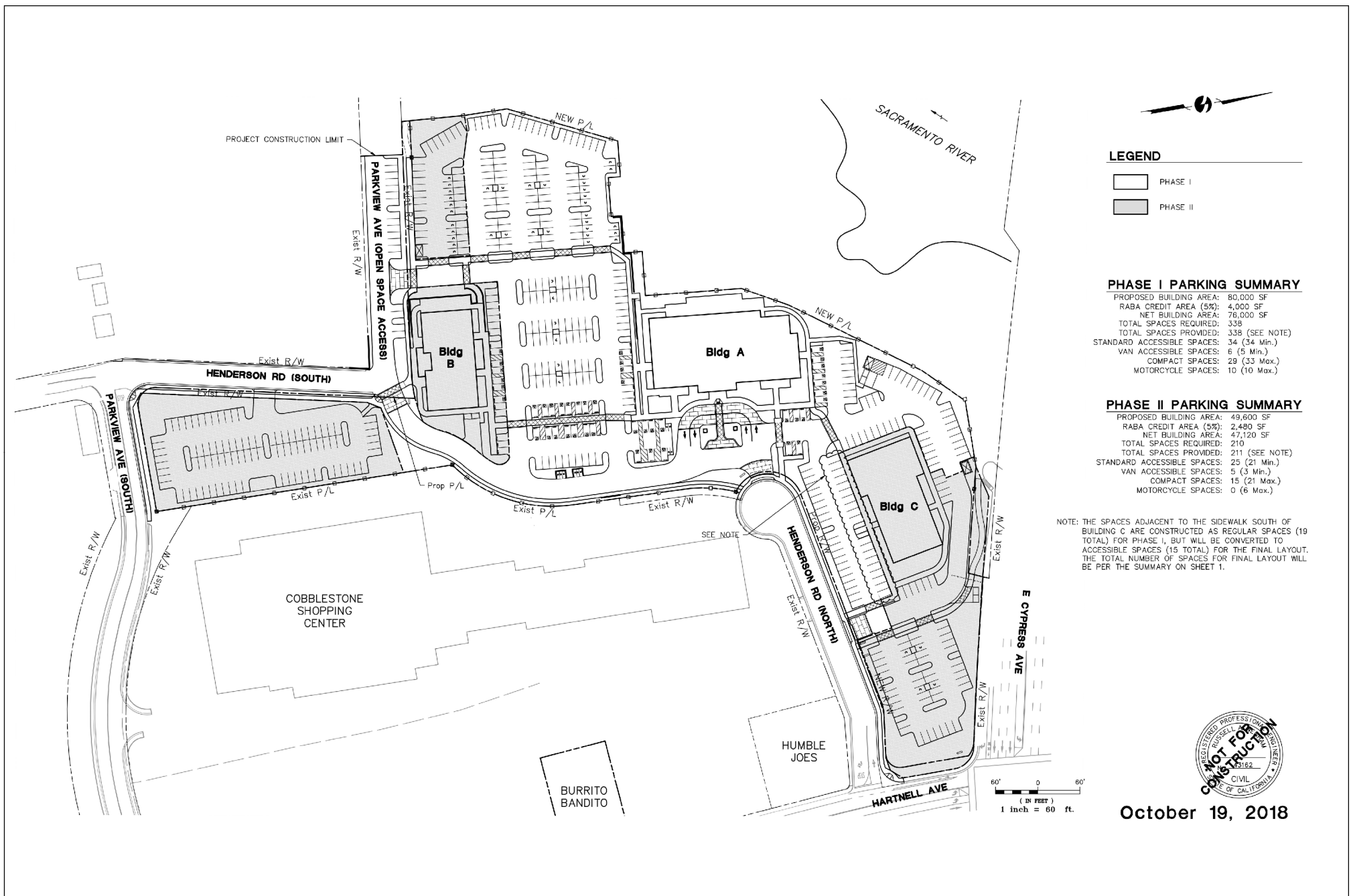
Figure 3-10a, KEY VIEW LOCATIONS, identifies the locations of the key views of the proposed project. Figures 3-10b, KEY VIEW 1 – BUILDINGS ‘A’ & ‘C’ and Figure 3-10c, KEY VIEW 2 – BUILDINGS ‘A,’ ‘C,’ & ‘B’ (partial) provide perspective renderings of “key views” of the project when viewed from across the Sacramento River, below the Cypress Avenue bridge and from the Cypress Avenue bridge, respectively.

The proposed buildings’ architecture provides a mixture of materials including, but not limited to; metal, stone, cement plaster, and glazing. The buildings and associated facades will have varying heights, sun shades, awnings, canopies, raised parapets with cornices, and other decorative fixtures to provide articulation to the building elevations which, along with varying natural earth tone colors and patterns, provide variation in the appearance of the buildings. The raised parapets with cornices also serve to provide screening of mechanical equipment. The canopies over the primary entrances to the buildings and the awnings along much of the building perimeters provide a more inviting “pedestrian” friendly scale than the maximum heights shown in Figures 3-8b through 3-8d. It should be noted that the “Public Facilities” (PF) zoning district designation does not limit the maximum building heights allowed as does the “General Commercial” (GC) or “General Office” (GO) zoning with limits of 45 and 50 feet, respectively.

Access and Parking

Table 3-6, SITE DEVELOPMENT SUMMARY, above, provides area and percentage estimates of the various proposed onsite and offsite project related improvements to Henderson Road (North), Parkview Avenue (South), Henderson Road (South), and Parkview Avenue (Open Space Access) ROWs under “City Roads / Public Right-of-Way.” The ROW width of 60 feet for the Parkview Avenue (Open Space Access) does not include the project related parking, driveway, sidewalk and landscaping proposed as a component of the project. The two public access points to the proposed project site are also easily accessible by foot from both Henderson Road (North) and Henderson Road (South).

Hardscape pathways are proposed throughout the project site linking buildings, parking areas to each other and providing access to the various streets, the Cypress Avenue bridge and the Henderson Open Space. In general, pathways are typically a minimum of 5 feet wide, with many of the pathways shown at 6 feet wide. In addition, certain areas of the project with pathways that are anticipated to have more pedestrian traffic have been strategically increased to 8 feet wide. These include an 8-foot wide northerly pathway that will link Henderson Road (North) to the stairs up to the Cypress Avenue bridge and/or continue to the Henderson Open Space. An 8-foot wide pathway is also proposed to link Building ‘A’ to the 8-foot wide pathway, basically connecting the sidewalk on the east side of Henderson Road (South), past Building ‘B’ and then connecting to the Henderson Open Space.



Dignity Health North State Pavilion Project

Project Phasing Plan

Figure 3-9



Dignity Health North State Pavilion Project

Key View Locations

Figure 3-10a



VIEW A
FROM ACROSS RIVER, BELOW BRIDGE



Dignity Health North State Pavilion Project

Key View 1 - Buildings 'A' & 'C'

Figure 3-10b



VIEW B
FROM CYPRESS BRIDGE



Dignity Health North State Pavilion Project

Key View 2 - Buildings 'A', 'C', & 'B' (partial)

Figure 3-10c

Per RMC requirements, the proposed project requires 576 parking spaces based on one space per 225 square feet of building. However, due to the Redding Area Bus Authority (RABA) "Bus Stop/Transportation Facility Credit," a parking space credit of 28 spaces is realized, thereby reducing the number of spaces required to 548. The credit allows up to a five percent reduction when a bus stop is located within 400 feet of the use. The nearest Route 5 bus stop is located on the west side of Hartnell Avenue, south of Henderson Road, approximately 200 feet from the proposed project.

An estimated 70 ADA accessible parking spaces will be provided of which 11 are van accessible. In addition, 33 of the parking spaces will have electric vehicle charging stations, and 44 spaces will be preferential parking for clean air vehicles. Onsite bicycle parking (28 secured bicycle racks) will be provided onsite.

Phase 1 provides the 338 parking spaces required of which 34 are standard accessible spaces, 6 are van accessible, 29 are compact spaces and 10 are motorcycle spaces. Phase 2 requires 210 parking spaces and 211 are provided. Standard accessible spaces total 25, 5 are van accessible, 15 are compact spaces and there are no motorcycle spaces proposed for Phase 3. It should be noted that the 19 parking spaces adjacent to the sidewalk south of Building 'C' are constructed as regular spaces for Phase 1 but will be converted to 15 accessible spaces as part of Phase 2.

Landscaping

The proposed landscape plan breaks the project up into distinct areas in an attempt to provide landscaping that both complements the proposed improvements as well as complementing the adjacent open space areas. Figure 3-11a, LANDSCAPE LAYOUT, provides a breakdown of landscape areas and the proposed plant palette is provided in Figure 3-11b, PLANT LIST. Figures 3-11c through 3-11f, PLANTING PLAN E-1 through E-4, provides enlarged layouts of the landscape plan. The project proposes approximately 92,100 square feet of landscaped areas including parking islands, perimeter landscaping, and internal campus landscaping. As illustrated on Figure 3-11a, the project proposes the planting of approximately 224 trees throughout the site to enhance the site's aesthetics.

The Valley Oak (*Quercus lobata*), a low water user, is the predominant tree proposed with 47 trees proposed. The Valley Oak can grow between 60 and 100 feet in height (approximately two to three feet per year) at full maturity with approximate crown coverage of 50 feet. In addition, the onsite trees at maturity will provide approximately 35 percent of shade coverage within the parking areas.

Other landscaping proposed throughout the site will include native and ornamental shrubs, grasses and other groundcovers. The utilization of native shrubs will ensure the landscaping is low-maintenance and will conserve water. Street cross-sections depicting how landscaping is integrated into adjoining roadway improvements are provided in Figure 3-6b and Figure 3-6c.

The proposed project has been designed to be consistent with the State of California's Model Water Efficient Landscape Ordinance (MWELO), adopted January 1, 2010 and updated on July 15, 2015 by the California Water Commission. Landscape irrigation will include automatic irrigation controller with soil moisture sensors/rain sensors; run-off prevention, low head drainage, and over spray; utilization of low volume/water efficient drip and rotary heads. Figure 3-11g, IRRIGATION ZONES, identifies the landscape areas throughout the site that have low, low to medium, medium, and high-water use zones. The high-water use zones are very limited turf areas.

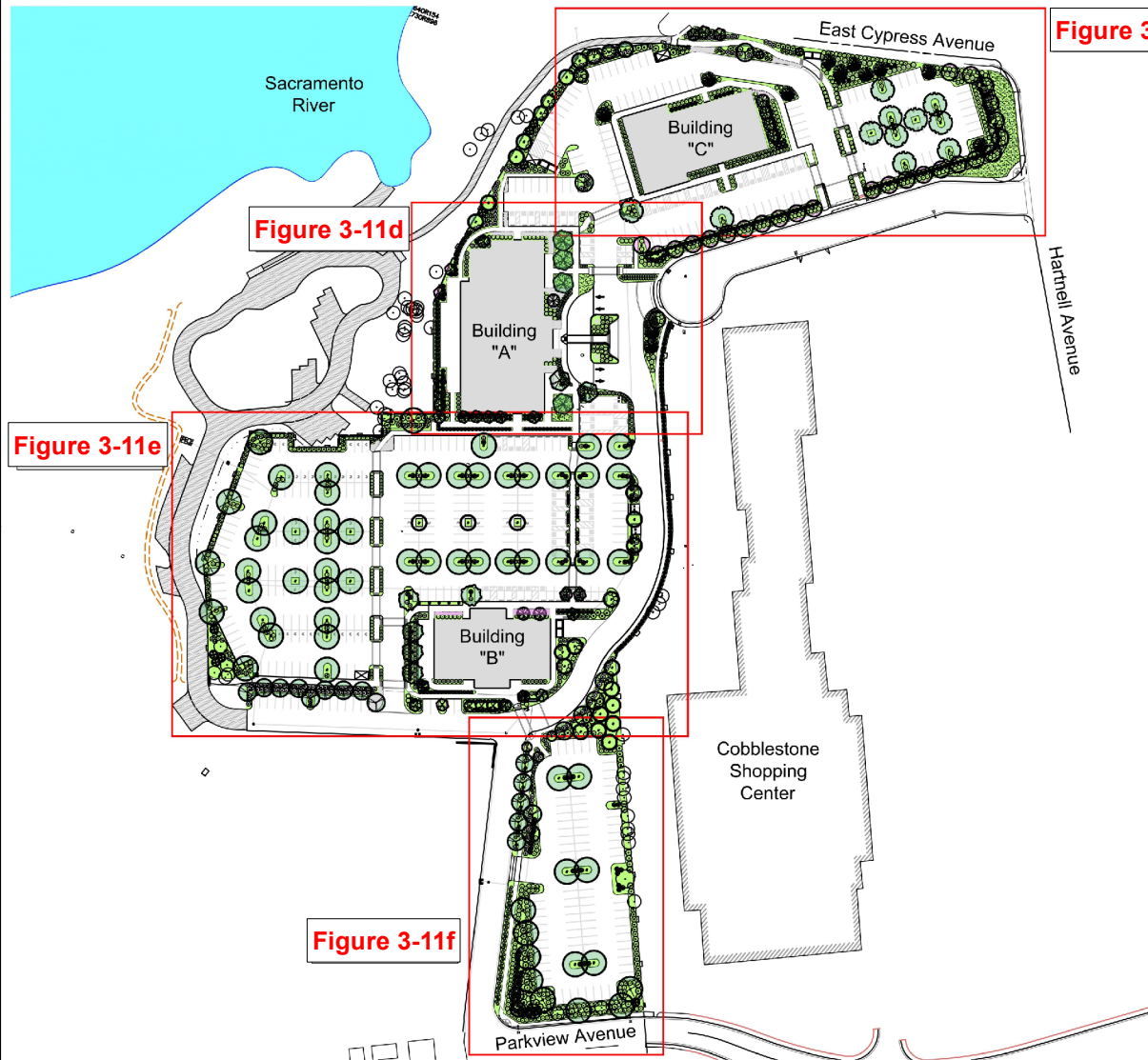


Figure 3-11c

Figure 3-11d

Figure 3-11e

Figure 3-11f

TOTAL LANDSCAPE AREA: 92,100 square feet

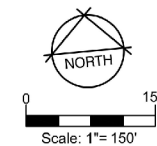
LANDSCAPE NOTES

- Landscaping is designed to transition from the commercial style along Hartnell Avenue to the natural feel along the Sacramento River.
- Plant material has been selected based on water use, esthetic factors, security, longevity, and low maintenance needs.
- The landscape and irrigation design will conform to the requirements of the California Model Water Efficient Landscape Ordinance and the City of Redding.
- Proposed irrigation includes in-line drip emitters (surface or subsurface installation) and high-efficient pop-up sprinklers.
- Irrigation controls will be weather based. The option of central control will be offered.

LANDSCAPE SUMMARY TABLE

Parking Spaces: 549
 Required Parking Trees: $549 \div 4 = 138$
 Parking Trees Shown: 142

Required Parking Landscaping: $549 \times 60 = 32,940$ SF
 Parking Landscaping Shown: 42,380 SF



11/13/2018



PLANT LIST

CODE	SIZE	BOTANICAL NAME	COMMON NAME
------	------	----------------	-------------

TREES

AF	15 gal	Acer freemanii 'Jeffersred'	Autumn Blaze Maple
AP	15 gal	Acer palmatum	Japanese Maple
CD	15 gal	Calocedrus decurrens	Incense Cedar
CC	15 gal	Cercis canadensis	Eastern Redbud
CO	15 gal	Cercis occidentalis	Western Redbud
CT	15 gal	Chitalpa tashkentensis	Chitalpa
LN	15 gal	Lagerstroemia x 'Natchez'	Crape Myrtle
NO	15 gal	Nerium oleander	Oleander
PC	15 gal	Pistacia chinensis	Chinese Pistache
PR	15 gal	Platanus racemosa	California Sycamore
QD	15 gal	Quercus douglasii	Blue Oak
QL	15 gal	Quercus lobata	Valley Oak
QW	15 gal	Quercus wislizenii	Interior Live Oak
SS	15 gal	Sequoia sempervirens	Coast Redwood
ZS	15 gal	Zelkova serrata	Sawleaf Zelkova

SHRUBS

AG	5 gal	Abelia x grandiflora 'Edward Goucher'	Glossy Abelia
AD	5 gal	Arctostaphylos densiflora 'Harmony'	Harmony Manzanita
CH	5 gal	Cistus x hybridus	White Rockrose
DI	5 gal	Dietes iridioides	Fortnight Lily
HS	1 gal	Helictotrichon sempervirens	Blue Oat Grass
LC	5 gal	Loropetalum chinense 'Ruby'	Fringe Flower
MC	1 gal	Muhlenbergia capillaris 'Lenca'	Regal Mist Pink Muhly
MR	5 gal	Muhlenbergia rigens	Deer Grass
NC	5 gal	Nandina domestica 'Compacta'	Heavenly Bamboo
NF	1 gal	Nandina domestica 'Fire Power'	Heavenly Bamboo
PT	5 gal	Parthenocissus tricuspidata 'Veitchii'	Boston Ivy
RC	5 gal	Rhamnus californica 'Eve Case'	Coffeeberry
RI	5 gal	Rhaphiolepis indica 'Indian Princess'	Indian Princess Indian Hawthorne
RU	5 gal	Rhaphiolepis umbellata 'Minor'	Dwarf Yedda Hawthorne Standard
RF	5 gal	Rosa x 'Flower Carpet Appleblossom'	Groundcover Rose

Shade Shrubs			
	5 gal	Camellia sasanqua	Sasanqua Camellia
	5 gal	Mahonia aquifolium	Oregon Grape
	5 gal	Viburnum davidii	David Viburnum

GROUND COVER

AE	1 gal	Arctostaphylos x 'Emerald Carpet'	Emerald Carpet Manzanita
EK	1 gal	Erigeron karvinskianus	Santa Barbara Daisy
JS	1 gal	Juniperus sabina 'Buffalo'	Buffalo Juniper
RO	1 gal	Rosmarinus officinalis 'Huntington Carpet'	Rosemary
TJ	1 gal	Trachelospermum jasminoides	Star Jasmine
ZC	1 gal	Zauschneria californica	California Fuchsia

Sun Accent

	1 gal	Achillea millefolium	Yarrow
	1 gal	Lavandula stoechas	Spanish Lavender
	1 gal	Salvia chamaedryoides	Germande Sage
	1 gal	Santolina chamaecyparissus	Lavender Cotton

Shade accent

	5 gal	Aspidistra elatior	Cast Iron Plant
	1 gal	Bergenia cordifolia	Heartleaf Bergenia
	1 gal	Helleborus orientalis	Lenten Rose
	1 gal	Liriope muscari	Lily Turf
	1 gal	Loropetalum chinense 'Purple Pixie'	Purple Pixie Loropetalum

Turf

	sod	90/10 mix of Turf type Tall Fescue and Kentucky Bluegrass
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Mulch

		Fir Walk on Bark over pre-emergent herbicide
		Crushed river run stone over filter fabric
		River cobbles over filter fabric

11/13/2018



Dignity Health North State Pavilion Project

Plant List

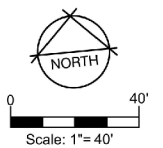
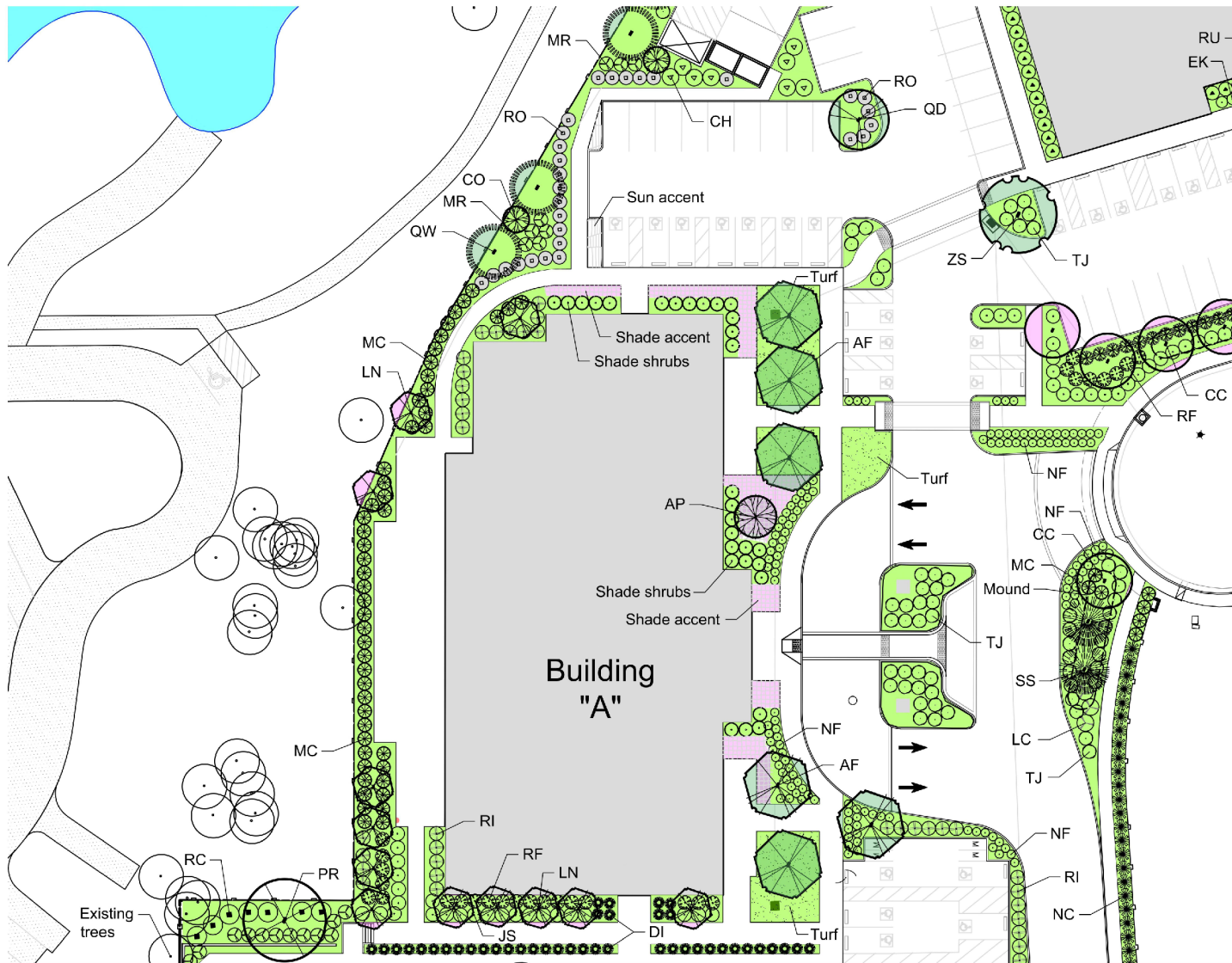
Figure 3-11b



Dignity Health North State Pavilion Project

Planting Plan E-1

Figure 3-11c



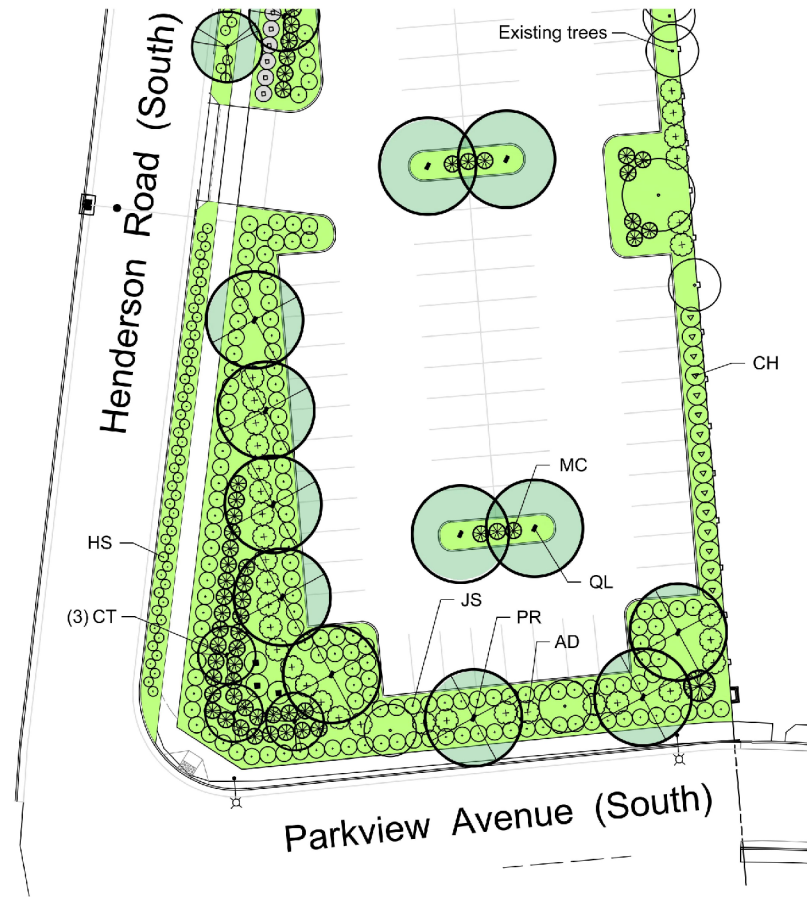
11/13/2018



Dignity Health North State Pavilion Project

Planting Plan E-2

Figure 3-11d



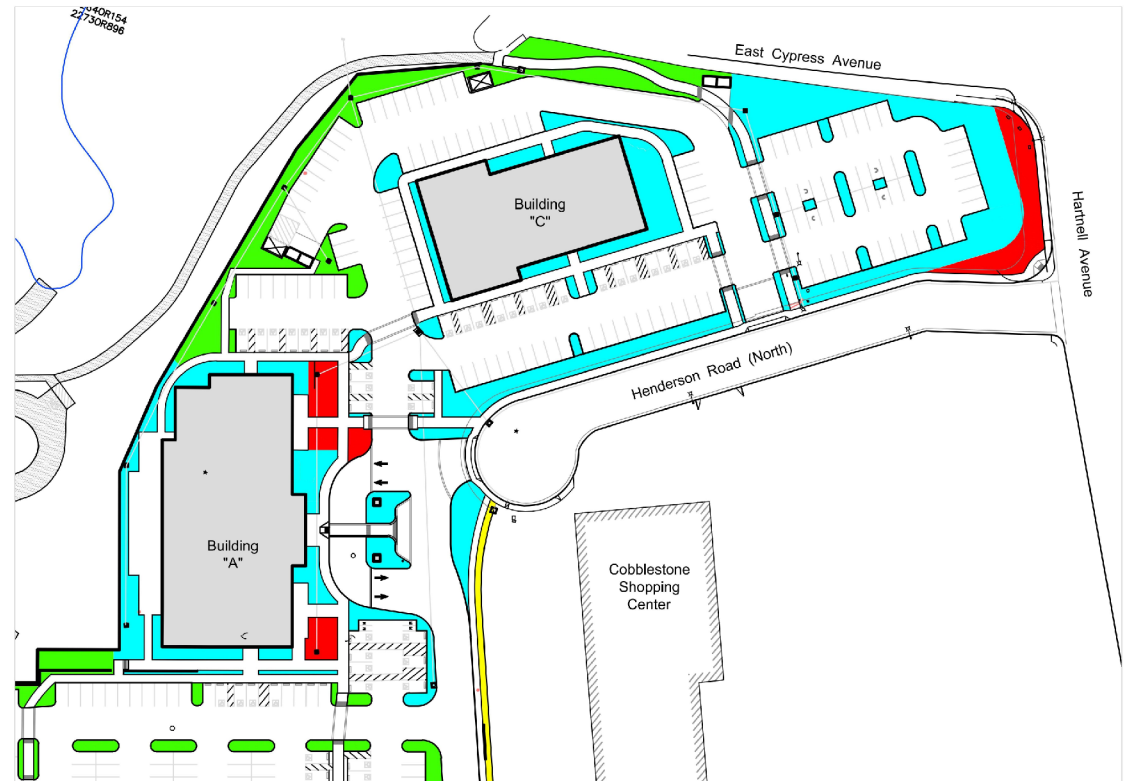
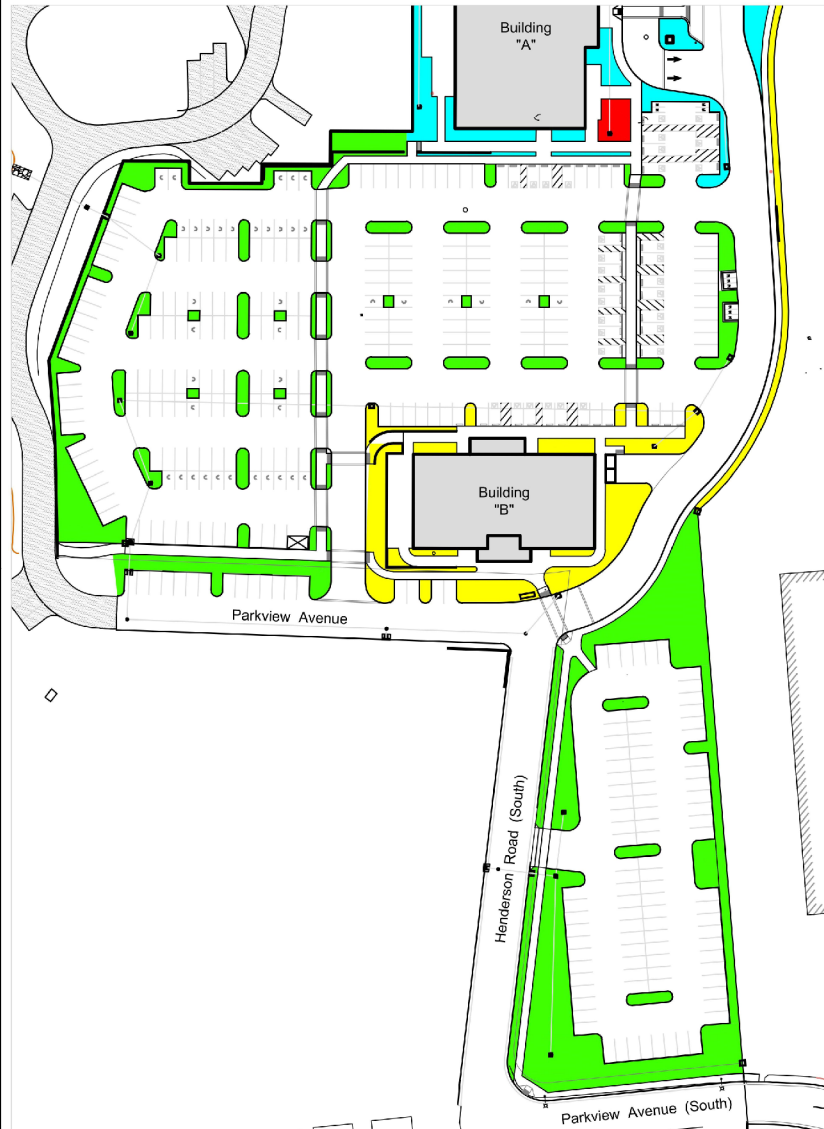
0 40'
Scale: 1"= 40'
11/13/2018



Dignity Health North State Pavilion Project

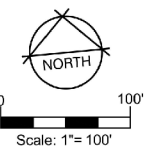
Planting Plan E-4

Figure 3-11f



IRRIGATION ZONES

- Low water use
- Low to medium water use
- Medium water use
- High water use (turf areas)



11/13/2018



Dignity Health North State Pavilion Project

Irrigation Zones

Figure 3-11g

Tree Removal & Retention

Project implementation would result in the redevelopment of approximately 8.8 acres of existing urban habitat. The urban classification is not considered natural feature of the landscape and is not identified and managed as a special-status natural community. However, the most important biological component of the onsite urban habitat is its mature trees, which may provide nesting habitat for migratory birds and roosting habitat for bats. In addition, approximately 0.4 acres of riparian habitat will be removed from the westernmost extension of the project site. Due to the disturbed nature onsite and the high level of human activity, the onsite riparian habitat does not represent a high-quality occurrence of the community type.

Figure 3-12, PROPOSED TREE REMOVAL PLAN, identifies that of the 82 mature trees with diameters greater than 6 inches at breast height (dbh), a total of 58 trees will be removed (refer to Table 3-7, TREE REMOVAL AND RETENTION, for a summary of removal and retained trees by type and habitat classification).

**Table 3-7
TREE REMOVAL AND RETENTION**

Tree Type	Total Onsite	Trees Retained		Trees Removed		
		Urban Habitat	Riparian Habitat	Urban Habitat	Riparian Habitat	Total Removed
Valley Oak	20	4	1	14	1	15
Freemont Cottonwood	37	8	-	9	20	29
Interior Live Oak	8	0	3	5	-	5
Tree of Heaven	7	2	--	5	-	5
Chinese Pistache	1	0	-	1	-	1
California Sycamore	2	1	-	1	-	1
Mulberry	1	0	-	1	-	1
Grey Pine	1	0	-	1	-	1
Black Willow	4	4	-	0	-	0
Japanese Privet	1	1	-	0	-	0
Totals	82	20	4	37	21	58

Source: GHD. *Proposed Tree Removal Plan*. November 2018.

None of the trees are considered heritage or landmark trees. Due to grading associated primarily with parking lot improvements, the greatest number of trees to be removed is an estimated 29 Fremont Cottonwood trees. Overall, the proposed project will minimally encroach into Valley Foothill Riparian areas where the majority of the offsite cottonwoods exist.

Parking Lot and Security Lighting

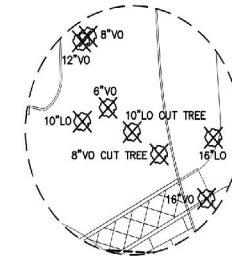
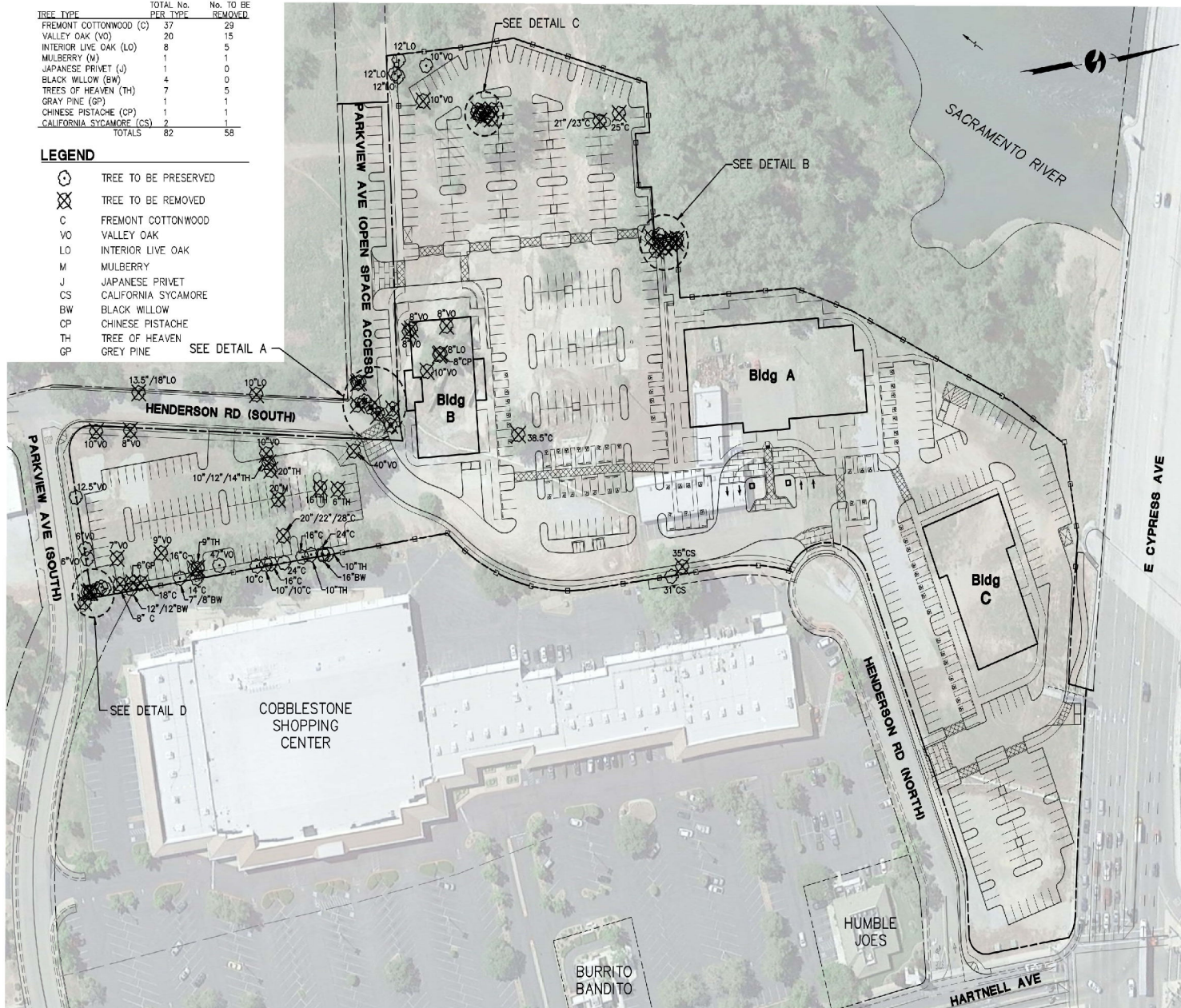
Parking lot lighting will include exterior pole-mounted light standards (maximum 25 feet high) located throughout the site to provided safety and security lighting. The light standards and additional wall-mounted light fixtures on building structures will be used to ensure safety of the public and safe onsite pedestrian and vehicular circulation. In accordance with RMC §18.40.090, *Lighting*, all parking area lighting, including building and pole-mounted lighting will be fully shielded and directed downward to prevent objectionable light at, or glare across, the property line and public ROW. Minimum security lighting will operate from dusk to dawn every day of the week.

SUMMARY OF TREE IMPACTS

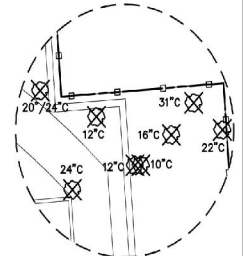
TREE TYPE	TOTAL No. PER TYPE	No. TO BE REMOVED
FREMONT COTTONWOOD (C)	37	29
VALLEY OAK (VO)	20	15
INTERIOR LIVE OAK (LO)	8	5
MULBERRY (M)	1	1
JAPANESE PRIVET (J)	1	0
BLACK WILLOW (BW)	4	0
TREES OF HEAVEN (TH)	7	5
GRAY PINE (GP)	1	1
CHINESE PISTACHE (CP)	1	1
CALIFORNIA SYCAMORE (CS)	2	1
TOTALS	82	58

LEGEND

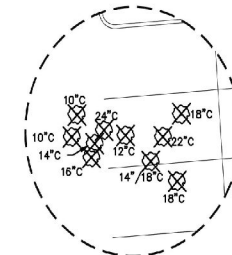
-  TREE TO BE PRESERVED
-  TREE TO BE REMOVED
- C** FREMONT COTTONWOOD
- VO** VALLEY OAK
- LO** INTERIOR LIVE OAK
- M** MULBERRY
- J** JAPANESE PRIVET
- CS** CALIFORNIA SYCAMORE
- BW** BLACK WILLOW
- CP** CHINESE PISTACHE
- TH** TREE OF HEAVEN
- GP** GREY PINE



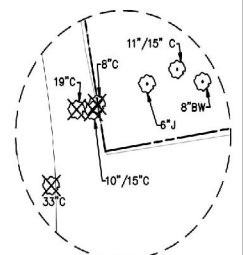
DETAIL A
SCALE: 1"=20'



DETAIL B
SCALE: 1"=10'



DETAIL C
SCALE: 1"=10'



DETAIL D
SCALE: 1"=10'

60' 0 60'
(IN FEET)
1 inch = 60 ft.



November 12, 2018



Dignity Health North State Pavilion Project

Proposed Tree Removal Plan

Figure 3-12

Grading

Site topographic elevations range from approximately 497 feet above msl in the northeastern corner of the site adjacent to Hartnell Avenue to approximately 467 feet above msl in the southern portion of the site in the area of Parkview Avenue (South). Overall, the site gently slopes in a western direction towards the Sacramento River with slopes varying between 1.7 and 3.3 percent.

Grading will require excavations for footing and foundations varying from 2 to 4 feet to accommodate Building 'C' located northeast of Building 'A' and parallel to Henderson Road (North). Excavations between 5 and 10 feet will be required for Building 'A.' For Building 'B', fills of up to 5 feet will be necessary. In the southern area of the site adjacent to the Henderson Open Space, fills of one to four feet will be required and in the northern area, grading will occur with some cuts and fills of up to two feet in and around the area of Building 'C' (refer to figures 3-13a and 3-13b, GRADING AND DRAINAGE PLANS).

To the maximum extent feasible, the earthwork will be balanced between cut and fill. Maximum excavations are estimated at 10 feet and maximum fills at 12 feet. The maximum amount of earthwork will be approximately 30,000 cubic yards (CYs) of which 15,000 CYs will be cut and 15,000 CYs of fill.

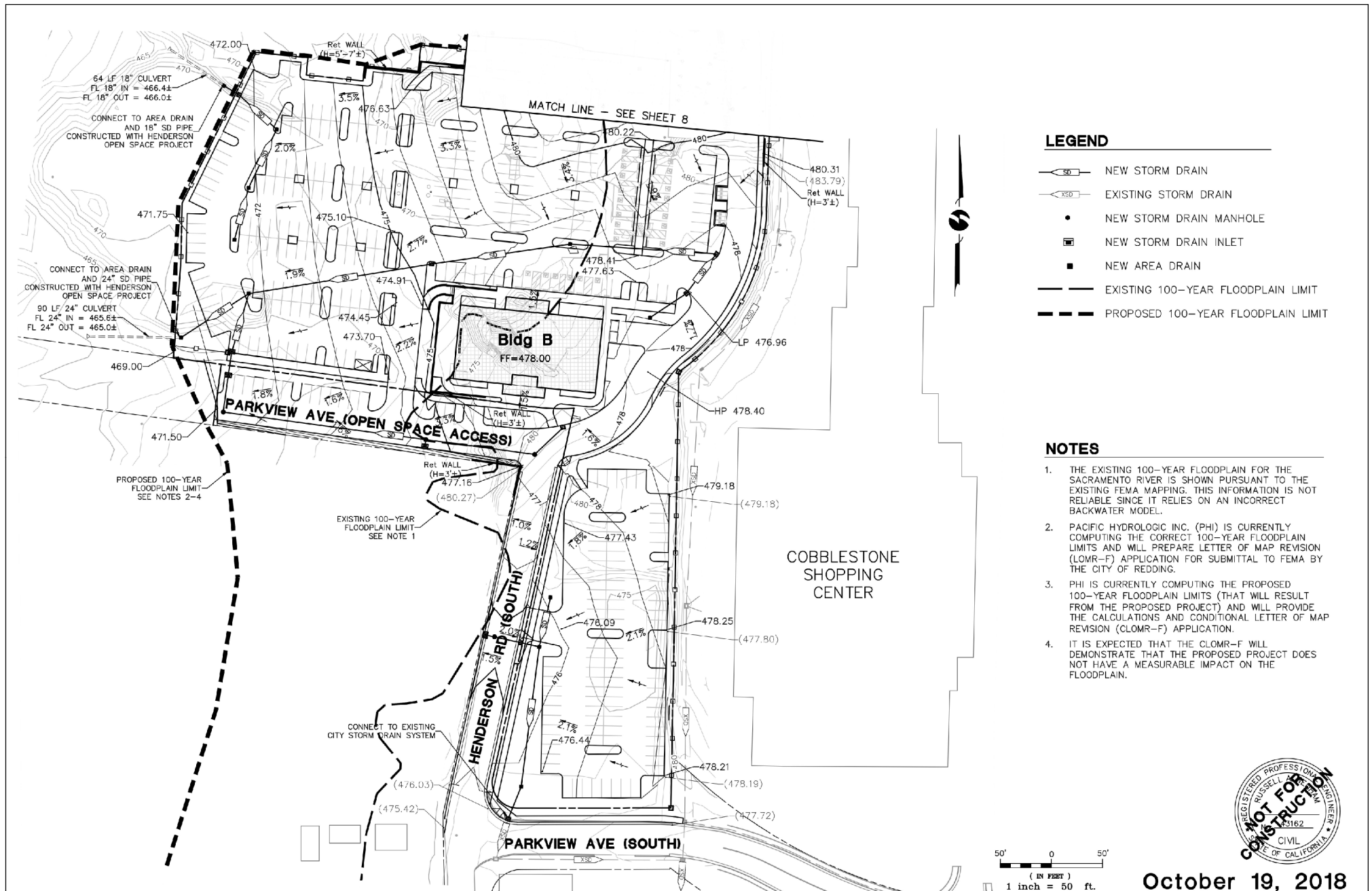
Floodplain

Approximately 3.58 acres of the proposed project's western parking lot area, south of Building 'A' and west of Building 'C' abutting the Henderson Open Space area, will encroach into the currently mapped FEMA 100-year floodplain as shown on Figures 3-13a and 3-13b, GRADING AND DRAINAGE PLANS. The City has a "no rise" policy requiring that new development not encroach within the 100-year floodplain and if it does, that the project applicant demonstrate that the new encroachment will not increase the water surface elevation during the most probable 100-year flood either upstream or downstream.

A formal analysis was completed in February 2016 that "determined that construction of the parking lot as presently anticipated will not increase the water surface elevation of the extent of inundation during the most probable 100-year flood." However, "a substantial portion of the parking lot is identified within the FEMA designated floodway, an area reserved to convey the FEMA Base Flood (FEMA estimate of the most probable 100-year flood).

Subsequent analysis determined that the FEMA floodway was incorrectly delineated and that "the parking lot will be entirely in the floodway fringe, that area between the floodway and the flood limit, rather than within the floodway." To make this correction a Letter of Map Revision (LOMR) must be prepared that meets the study and mapping requirements of FEMA. It should be noted that the high-water releases from Shasta Dam in February and March 2018 did not affect the analysis.

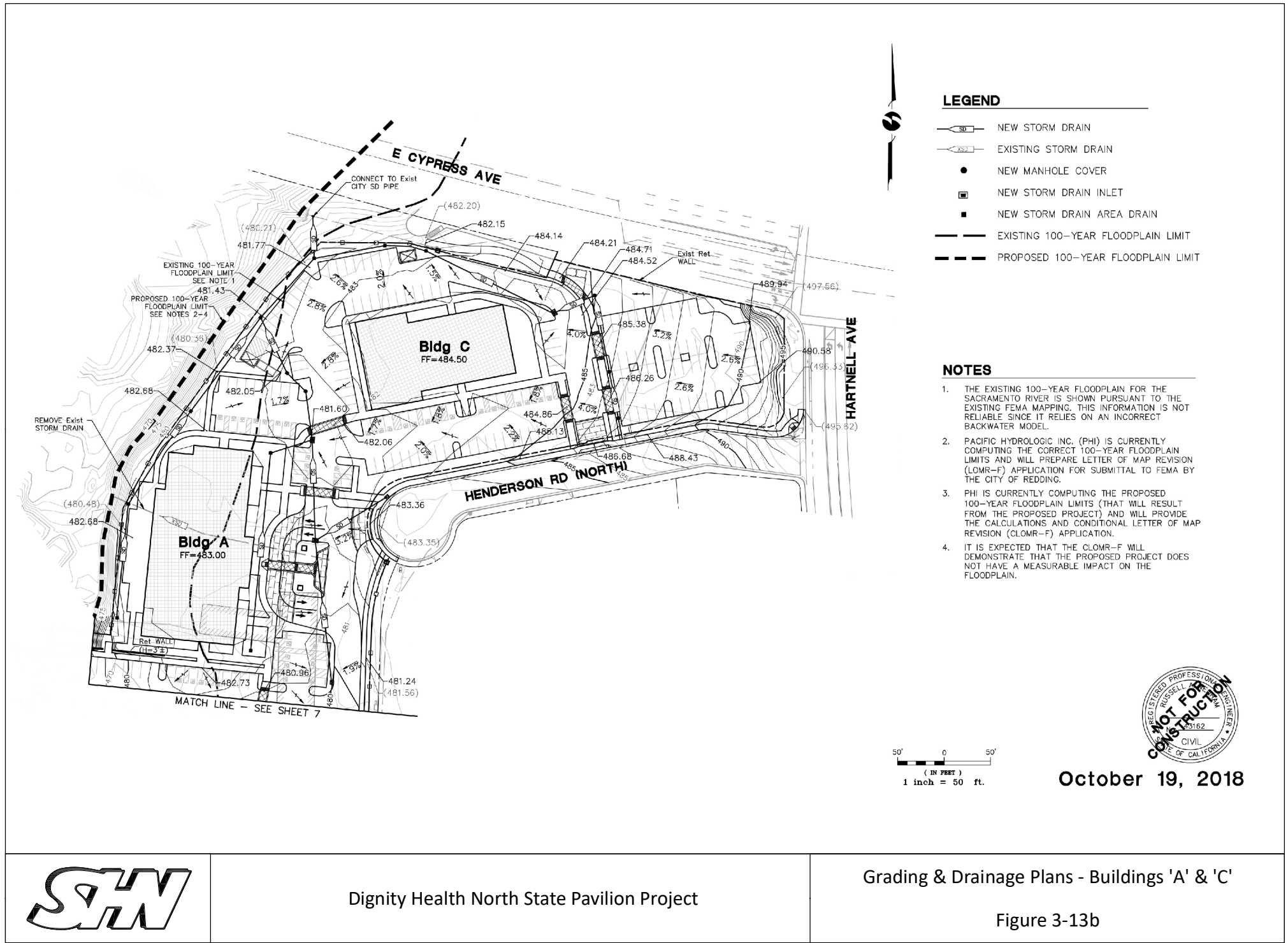
A Conditional Letter of Map Revision based on Fill (CLOMR-F) will need to be approved prior to issuing a grading permit for fill in the floodway fringe. A LOMR dated February 14, 2017 was prepared and subsequently submitted to FEMA by the City after their review and approval. Once the LOMR is approved, the CLOMR-F will be prepared and submitted to FEMA.



Dignity Health North State Pavilion Project

Grading & Drainage Plans - Building 'B'

Figure 3-13a



Dignity Health North State Pavilion Project

Grading & Drainage Plans - Buildings 'A' & 'C'

Figure 3-13b

Storm Water Management

Portions of the existing site discharge as sheet flow towards the west, whereas within other areas of the site, storm water is captured by existing storm drain pipes or existing channels, which then generally discharge towards the west and towards the Sacramento River. Storm water from the proposed project site will discharge in a manner similar to existing conditions, with most of the storm water discharging to the west, towards the Sacramento River, at three distinct discharge locations. The upper area to the north flows into an existing 36-inch culvert, which discharges into an existing channel near the Sacramento River, just south of the Cypress Avenue bridge. Most of the lower area will also flow to the west and will connect to new culverts to be installed within the Henderson Open Space.

A small area at the south end of the project, adjacent to Henderson Road (South) and Parkview Avenue (South), will be discharged into the existing 15-inch City storm drain pipe near the intersection of Henderson Road (South) and Parkview Avenue (South). Refer to Figure 3-14, STORMWATER CONTROL PLAN, which identifies the impervious and pervious surfaces, drainage management areas and boundaries, proposed storm drain manholes or drainage inlets and a table providing post-construction Best Management Plan (BMP) calculations. Cross-sections of flow-through planter and bioretention area details are also illustrated.

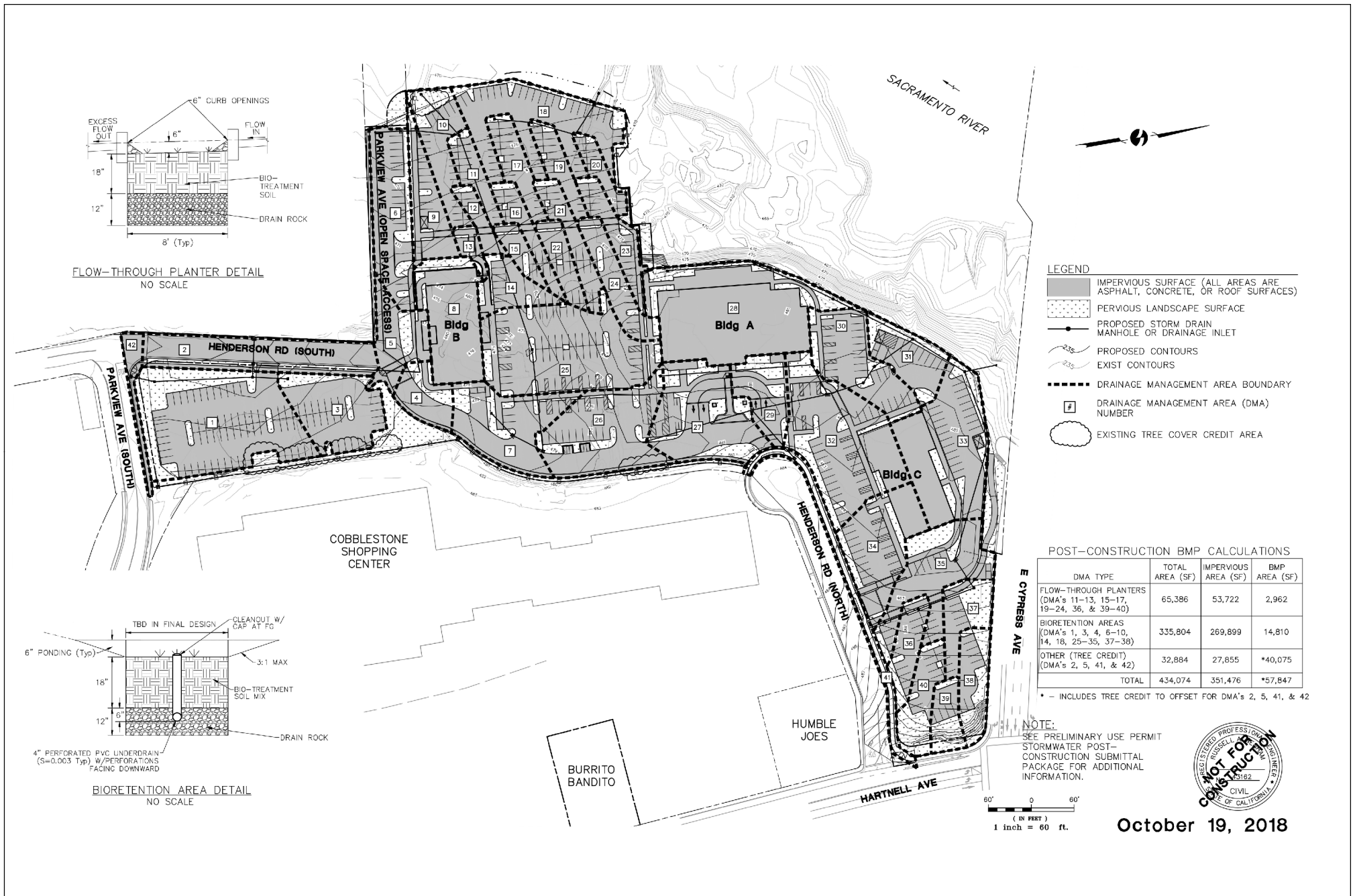
The proposed project will be approximately 77 percent impervious, which is a significant increase from existing approximate 27 percent impervious condition. Development of the project will result in 10-year, 25-year, and 100-year peak flows that are 38 cubic feet per second (cfs), 45 cfs, and 56 cfs, respectively. This amounts to an increase of 7 cfs, 8 cfs, and 10 cfs for the 10-year, 25-year, and 100-year peak flows, respectively.

Although the project increases peak flows, the increases discharge directly to City land and into the Sacramento River, so there are no downstream properties that will be adversely affected by the increased runoff. Therefore, the project is not subject to mitigation of increased peak flows.

A *Preliminary Use Permit Storm Water Post-Construction* package accompanied the entitlement application. Storm water management techniques include, but are not limited to, the construction of flow-through planters and bio-retention areas within drainage management areas throughout the project site as illustrated in Figure 3-14.

Utility Infrastructure Improvements and Service Provision

The proposed project will construct water, sanitary sewer, natural gas, electric, telephone, cable, and street light on and offsite utility infrastructure improvements, in addition to the storm water management improvements previously discussed to support the development (refer to Figure 3-15a, UTILITY PLAN – BUILDING ‘B’ and Figure 3-15b, UTILITY PLAN – BUILDING ‘A’ & ‘C’).



Dignity Health North State Pavilion Project

Stormwater Control Plan

Figure 3-14

Water and sewer services will be provided by the City of Redding and easily extended. Dignity will be connecting to an existing sewer along Cypress Avenue via a private onsite lift station. Storm water will be collected and detained onsite and transferred via an underground conveyance system to three connection points. One connection is to the City's storm-drain system located at the northwest corner of the project site and the other two will each tie into a proposed culvert at the west property line with the Henderson Open Space site and will daylight within the Henderson Open Space area. Dignity will be responsible to locate an onsite detention facility outside the building pad and provide detention facilities to accommodate the building and parking areas.

The City of Redding Solid Waste Division will collect solid waste. The proposed project will provide the required trash enclosures as shown on the site plan. Pacific Gas and Electric Company (PG&E) will provide natural gas service, and the City of Redding will provide electrical services. Three emergency backup generators with enclosures are proposed onsite as depicted on the site plan. Each generator will require one 500-gallon above ground diesel storage tank within the enclosure. AT&T can provide land line telephone services. Spectrum can provide cable services. Dignity will be responsible to coordinate the relocation and/or extension of existing gas, electric, and telephone lines to the buildings.

Traffic Improvements

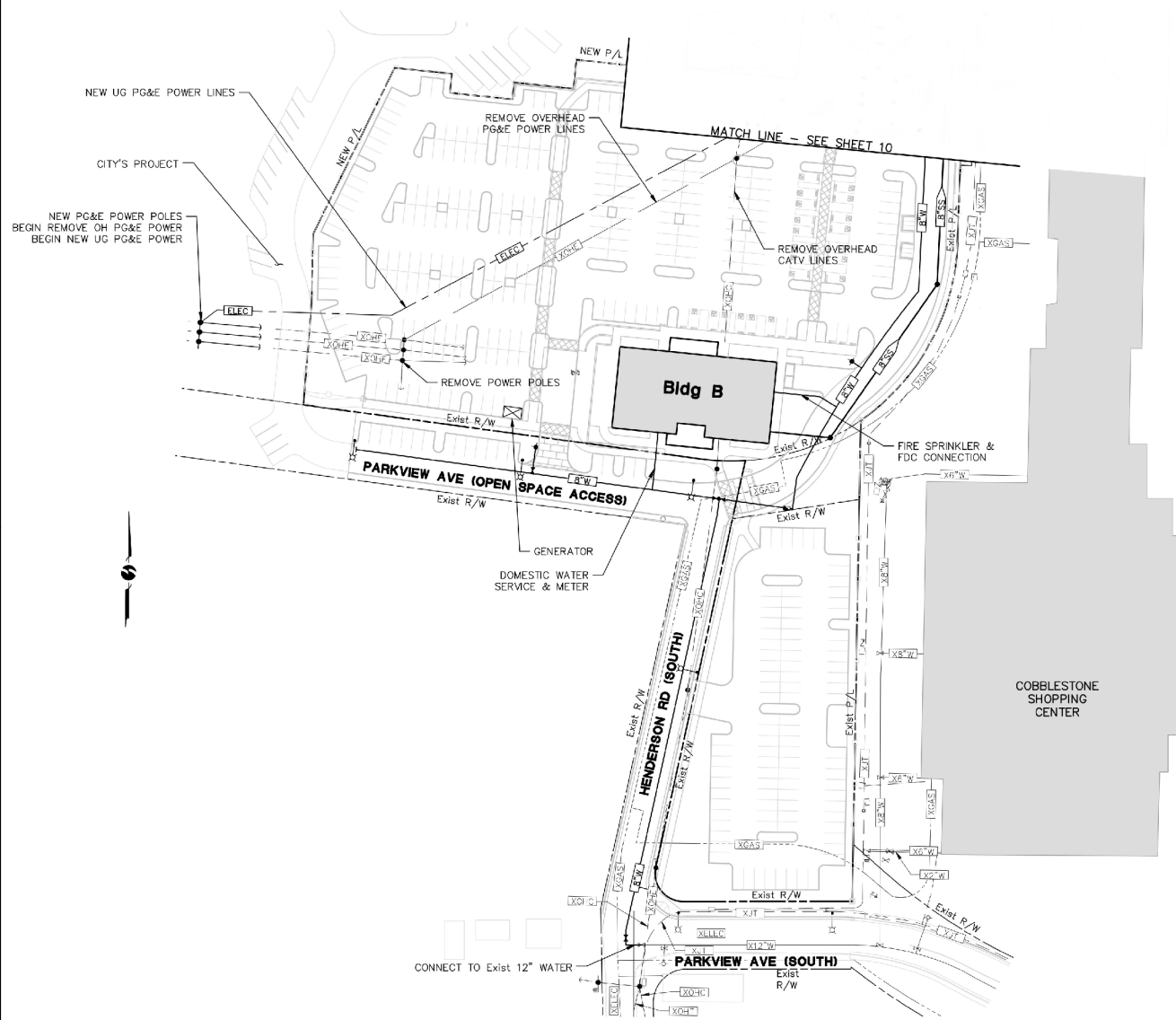
Based on the *Dignity Mercy Medical Center Redding – North State Pavilion Traffic Impact Analysis Report* (October 2018), proposed traffic mitigation measures for the project include:

- Two full access driveways to and from Hartnell Avenue to Henderson Road (North).
- One full access driveway at Parkview Avenue/ Henderson Road (South).
- The modification of the intersection of Henderson Road (North) and Hartnell Avenue to right-in-right-out only.
- At the Hartnell Avenue and Cypress Avenue intersection:
 - o Construction of a southbound left turn pocket.
 - o Construction of a southbound thru/right lane.
 - o Conversion of the intersection to an eight-phase traffic signal.

The traffic analysis determined that all project impacts will be mitigated through the payment of citywide traffic impact fees and the construction of the above improvements.

ECONOMIC CHARACTERISTICS

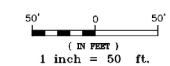
Development of the proposed project will have fiscal impacts on the City similar to other medical office facilities, in particular additional street maintenance, albeit minimal due to the amount of existing street to be utilized and improved, and the provision of law enforcement and fire services, which are not public utilities and are therefore funded by the City's general fund. The proposed project will generate revenue to fund capital improvements through the payment of development impact fees, including fire facilities, water, wastewater, parks and recreation, and traffic. Building permit fees will also be paid.



LEGEND

- XW— EXISTING WATER VALVE
- XW— NEW WATER VALVE
- XW— EXISTING FIRE HYDRANT
- XW— NEW FIRE HYDRANT
- XW— NEW BACKFLOW (RPP) DEVICE
- XW— NEW SANITARY SEWER MANHOLE
- XW— EXISTING POWER POLE
- XW— NEW POWER POLE
- XW— NEW STREET LIGHT
- XW— EXISTING SANITARY SEWER
- XW— NEW SANITARY SEWER
- XW— EXISTING WATERLINE
- XW— NEW WATERLINE
- XW— EXISTING GAS
- XW— EXISTING OVERHEAD CABLE TV
- XW— EXISTING OVERHEAD TELEPHONE
- XW— EXISTING OVERHEAD ELECTRIC
- XW— EXISTING UNDERGROUND ELECTRIC
- XW— EXISTING UNDERGROUND JOINT TRENCH (ELEC, TEL, & CATV)

NOTE: FIRE DEPARTMENT CONNECTIONS SHALL BE LOCATED WITHIN 40' OF A FIRE HYDRANT, AND AS APPROVED BY THE CITY OF REDDING FIRE DEPARTMENT.



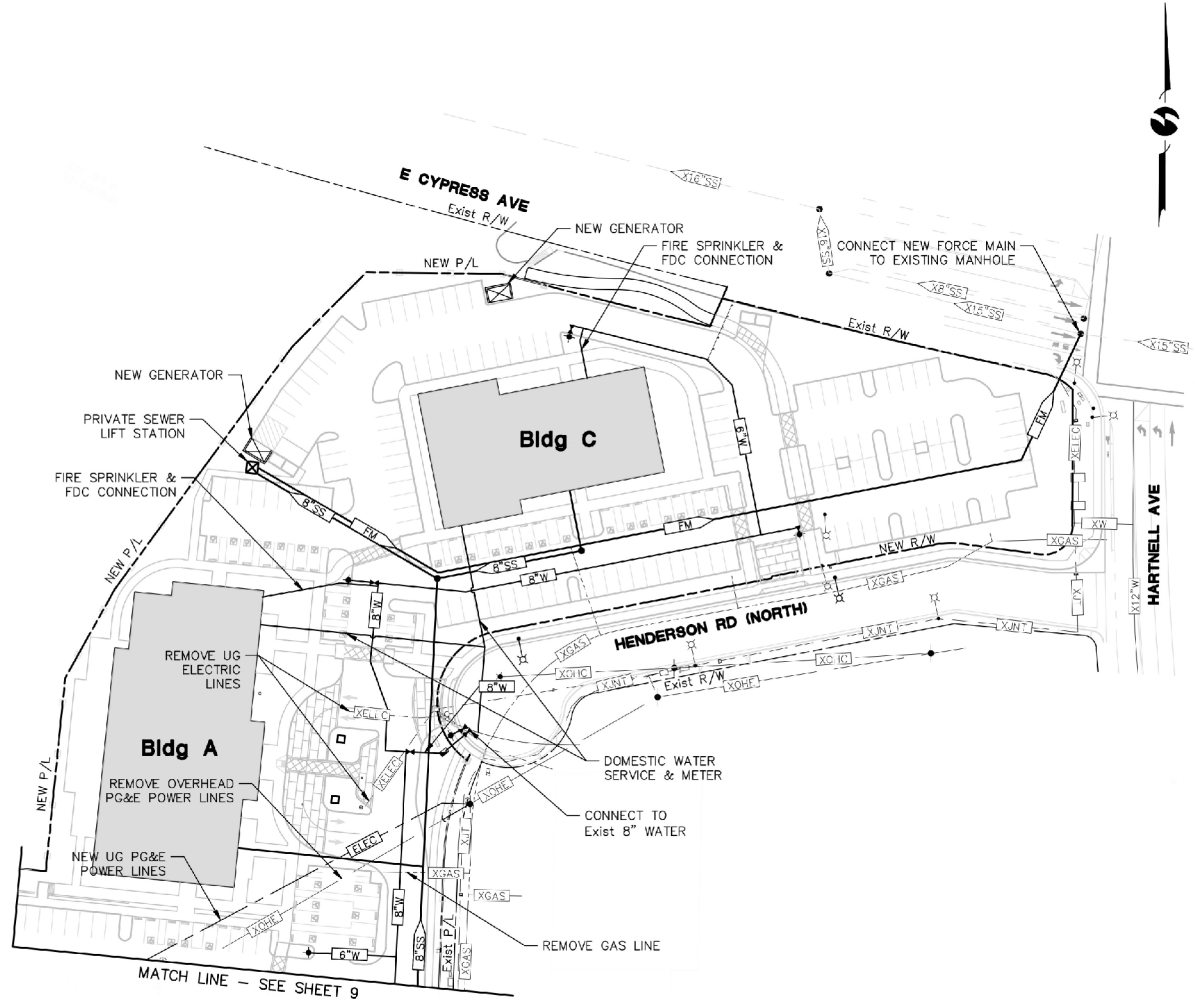
October 19, 2018



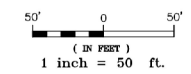
Dignity Health North State Pavilion Project

Utility Plan - Building 'B'

Figure 3-15a



LEGEND	
	EXISTING WATER VALVE
	NEW WATER VALVE
	EXISTING FIRE HYDRANT
	NEW FIRE HYDRANT
	NEW BACKFLOW (RPP) DEVICE
	NEW SANITARY SEWER MANHOLE
	EXISTING POWER POLE
	NEW POWER POLE
	NEW STREET LIGHT
	EXISTING SANITARY SEWER
	NEW SANITARY SEWER
	EXISTING WATERLINE
	NEW WATERLINE
	EXISTING GAS
	EXISTING OVERHEAD CABLE TV
	EXISTING OVERHEAD TELEPHONE
	EXISTING OVERHEAD ELECTRIC
	EXISTING UNDERGROUND ELECTRIC
	EXISTING UNDERGROUND JOINT TRENCH (ELEC, TEL, & CATV)



October 19, 2018



Dignity Health North State Pavilion Project

Utility Plan - Buildings 'A' & 'C'

Figure 3-15b

With respect to employment, the proposed project will require a peak construction workforce of 35 to 40 workers. Construction workers are expected to travel to the project site from various locations throughout the local area and northern California, in general. The number of workers expected to relocate to the surrounding area will not be substantial since the project applicant has historically used local general contractors and construction trades for the development of their facilities. Because construction will be temporary, occurring over a four-year period, it is not likely that it will require substantial numbers of people to relocate to the City of Redding or other areas of Shasta County.

The proposed project is estimated to provide employment opportunities for up to 180 persons which will include physicians, nurses, technicians, clerical, security, and custodial personnel. The addition of 180 full time staff would potentially increase the City of Redding population by 428 persons, assuming the full-time staff would relocate from outside the local area with families (based on 2.38 average household size for Redding, DOF, 2018). It is estimated that the facilities will accommodate up to 180 persons availing themselves of the services the wellness center will offer. It is not known how many employees and associated personnel will be from out of the area who will relocate to Shasta County and or either of the three cities.

HOUSING

It is assumed that the majority of employees will live in owner-occupied housing. The 180 new employees will either be permanent residents in the City of Redding, or in nearby unincorporated areas of Shasta County in either single-family or multi-family housing. The City has an estimated 39,679 housing units (27,116 single-family units), with a vacancy rate of 5.5 percent and the County as a whole has an estimated 78,745 housing units (57,355 single-family units), with a vacancy rate of 8.1 percent (refer to Section 5.11, POPULATION AND HOUSING).

PROJECT CONSTRUCTION

Construction of the proposed project is anticipated to begin by spring 2020 with build-out anticipated over a four-year period. Construction activities will include grubbing/clearing of the project site, cut/fill and compaction of soils, installation of utilities (e.g., underground power, sewer, water, telephone, and storm drainage facilities), construction of proposed buildings, paving and landscaping. Most of the proposed project area will be graded for building, parking, driveway and landscape areas. As previously noted, grading will be balanced, and the maximum amount of earthwork will be 30,000 CYs of which 15,000 CYs will be cut and 15,000 CYs of fill.

Phase 1 of the project includes demolition and removal of an existing 7,500 square-foot building and approximately 64,000 square feet of pavement. Phase 1 also includes mass grading of the entire 10.55-acre project site, and construction of Building 'A', interior roads and 338 parking spaces. Phase 1 construction would commence in 2020 and be complete by 2022. It is anticipated that Phase 1 construction would occur for 2 years. Phase 2 construction is assumed to commence in 2022, after completion of Phase 1. Phase 2 would include construction of Buildings 'B' and 'C' and the remaining 211 parking spaces. It is anticipated that Phase 2 construction would occur for 2 years.

Equipment used for construction will vary day-to-day depending on the activity, but will include scrapers/earthmovers, wheeled dozers, water trucks, forklifts, wheeled loaders, and/or motor graders. Refer to Section 5.10, NOISE, for a more thorough discussion of construction equipment. At the height of construction, up to 40 construction workers will commute to the site on a daily basis. The construction work force, construction equipment and material supply trucks will access the site primarily

via Henderson Road (North) for Phase 1 construction and Park View Avenue (South) and then Henderson Road (South) for Phase 2 construction.

The City's Grading Ordinance (RMC §16.12.120.H) limits grading permit authorized activities to between the hours of 7:00 AM and 7:00 PM, Monday through Saturday. No operations are allowed on Sunday. Construction activities, except for grading, will likely be limited to normal daytime hours (i.e., 6:00 AM to 7:00 PM on weekdays from May 15th through September 15th and from, 7:00 AM to 7:00 PM on weekdays from September 16th through May 14th, and from 9:00 AM to 8:00 PM on weekends and holidays in accordance with RMC §18.40.100). The proposed project does not require nighttime lighting for construction activities.

ENVIRONMENTAL CHARACTERISTICS

Environmental characteristics associated with the proposed project are discussed in detail in Section 5.0, DESCRIPTION OF ENVIRONMENTAL SETTING, IMPACTS, AND MITIGATION MEASURES.

3.5 PROJECT OBJECTIVES

PROJECT OBJECTIVES

State *CEQA Guidelines* (§15124[b]) require that the project description contain a statement of objectives that includes the underlying purpose of the project. The underlying purpose of the proposed project is to continue Dignity Health's long-standing commitment to providing high quality healthcare services to the City of Redding through a new "Wellness Center" where medical and mental health care professionals provide community residents and businesses centralized health services in one geographic location to improve overall physical and mental health.

The proposed project has the following objectives:

01. Maximize positive tax revenues to the City's General Fund, as well as support the City's economic development goals.
02. Provide for a comprehensively planned "Wellness Center" project in a campus-like setting whereby, the buildings are compatible with each other from a site planning, architectural, and landscape design perspective.
03. Provide the proposed project in a relatively centralized location within the City to facilitate efficient traffic utilization of existing arterials linking Interstate 5 and State Highways 44, 299, and 273 for access from throughout the City and Shasta County.
04. Promote the use of alternative modes of transportation by locating the site within close proximity to local bus routes and public bicycle and pedestrian facilities.
05. Locate the proposed project in an area in relatively close proximity to the City's main hospitals, Mercy Medical Center and Shasta Regional Medical Center, to coordinate services, as necessary.

06. Make efficient use of underutilized redevelopment land while creating a physical connection between the Wellness Center location and a healthy outdoor living experience.
07. Promote walking as a lifestyle by providing onsite and offsite pedestrian friendly infrastructure to the open space area to the west and shopping center, including restaurants and retail uses to the east.
08. Create new employment opportunities that contribute to improving the local economy while providing much needed physical and mental health and related educational services.
09. Provide a project that is sensitive to environmental issues, such as minimizing impacts to riparian areas to the west, conserving energy and encouraging alternative modes of transportation, while minimizing the extension of public services and utilities to the maximum extent feasible.

3.6 INTENDED USES OF THE EIR

This EIR provides an analysis of environmental impacts potentially resulting from implementation of the proposed project. The principal State *CEQA Guidelines* sections governing content of this document are §15120 through §15132 (Content of an EIR) and §15161 (Project EIR). This EIR serves as an informational document for the public and decision-makers, as identified in State *CEQA Guidelines* §15121, which states the following:

- *An EIR is an informational document which will inform public agency decision-makers and the public generally of the significant environmental effects of a project, identify possible ways to minimize the significant effects, and describe reasonable alternatives to the project. The public agency shall consider the information in the EIR along with other information which may be presented to the agency.*
- *While the information in the EIR does not control the agency's ultimate discretion on the project, the agency must respond to each significant effect identified in the EIR by making findings under §15091 and if necessary by making a statement of overriding consideration under §15093.*
- *The information in an EIR may constitute substantial evidence in the record to support the agency's action on the project if its decision is later challenged in court.*

State *CEQA Guidelines* §15161 defines a Project EIR as the most common type of EIR which examines the environmental impacts of a specific development project. Per State *CEQA Guidelines* §15161, the Project EIR “should focus primarily on the changes in the environment that would result from the development project. The EIR shall examine all phases of the project including planning, construction and operation.”

3.7 AGREEMENTS, PERMITS, AND APPROVALS

The City of Redding is considered the Lead Agency under CEQA and is responsible for reviewing and certifying the adequacy of this EIR. Responsible agencies are those agencies that have discretionary approval over one or more actions involved with the development of the proposed project site. Trustee agencies are state agencies having discretionary approval or jurisdiction by law over natural resources affected by the project. Prior to development of the proposed project, a number of discretionary

permits and approvals must be obtained, from local and State agencies, as listed below. This EIR will be relied on by the City and other responsible agencies when determining whether to issue discretionary approvals to implement the project. To implement the proposed project, the project applicant will need to obtain, at a minimum, the following discretionary permits/approvals.

CITY OF REDDING

- Certification by the City of Redding City Council that the Final EIR has been completed in compliance with CEQA and has been reviewed and considered by the decision-makers.
- Approval of a general plan amendment, rezone, issuance of a variance and use permit, and approval of a parcel map by the City of Redding.
- Recordation of the Final Parcel Map.
- Adoption of a CEQA Mitigation Monitoring and Reporting Program (MMRP).
- Issuance of encroachment permits by City of Redding for road work and other improvements that may be constructed in local road rights-of-way.
- Issuance of a grading permit.
- Approval of utility and public facility improvement plans.
- Issuance of building permits and associated payment of impact fees.
- Payment of development impact fees prior to issuance of occupancy permit(s).

Future required approvals and possible permitting requirements from other public agencies will be required. Upon completion of the environmental review process and prior to construction, the proposed project will be reviewed through standard City plan check procedures to verify that the project conforms to all applicable City design criteria.

OTHER LOCAL AGENCIES

- Payment of school impact fees to the Shasta County Department of Education.

STATE OF CALIFORNIA

- Issuance of a “dewatering permit” by the Regional Water Quality Control Board (RWQCB) should water need to be pumped from trenches to facilitate utility line construction or other below-ground construction.
- Issuance of a General Construction Activity Storm Water Permit by the RWQCB.

FEDERAL APPROVALS

- Federal Emergency Management Agency (FEMA) – Letter of Map Revision (LOMR) and Conditional Letter of Map Revision based on Fill (CLOMR-F)