



NORTH BUSINESS PARK SPECIFIC PLAN

City of Westlake Village - SP No. 2

Public Review Draft | March 2019



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PUBLIC REVIEW DRAFT

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A. Purpose and Background

The North Business Park Specific Plan is a City-initiated Specific Plan that demonstrates a clear vision and development framework for the future of an aging business park in the northern portion of the City. The Specific Plan is a comprehensive planning document and regulatory tool for implementing the City's General Plan and guiding development in this focused area through customized land use regulations and recommendations for public realm improvements.

As evidenced by development requests within the City over the past decade, the escalation of land values and regional growth pressures has resulted in a trend toward land use intensification within existing business districts, as well as new issues related to land use compatibility and the maintenance of community character and cohesiveness. In line with this trend, the North Business Park has gone through a transformation in the past two decades in response to market pressures to redevelop older, outdated facilities within a desirable trade area. As such, several of the older industrial properties in the southern portion of the business park have redeveloped with other uses, including the Four Seasons hotel, spa and wellness center, Dole corporate headquarters, Westlake Village Studios, Oaks Christian School, and Calvary Community Church. These investments have improved the image of the community as viewed from US Highway 101 and have caused interest among other property owners regarding what they might do with their properties.

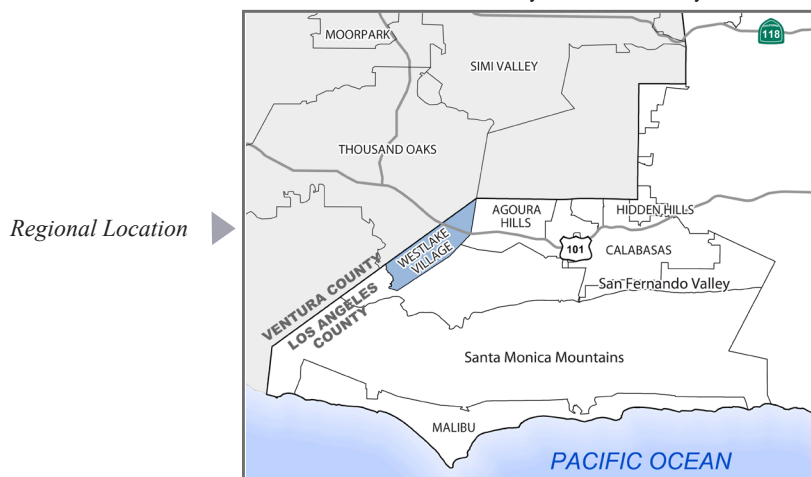
In response to these changes and property owner interest, the City initiated the development of a Specific Plan for the area in the mid-2000s. The Draft Specific Plan, which was never adopted, proposed zoning for higher density development, including housing, office and retail uses. As determined through preliminary environmental review, implementation of the Draft Specific Plan would result in projected traffic increases that could not be fully mitigated and were above the community's acceptance level. In addition, while the Specific Plan was being prepared, economic and physical

conditions changed, particularly with the development of the Shoppes at Westlake Village (Target Center) just east of the Specific Plan area, which lowered the market demand for some of the retail uses envisioned for the Specific Plan area. As a result, the City revisited the land use assumptions for future development and conducted further traffic and economic studies to achieve a build out scenario for the Specific Plan area that better balances economic feasibility with the goal of having acceptable intersection levels of service on the surrounding street system.

The purpose of the North Business Park Specific Plan is to provide a long-range strategy for revitalizing the Westlake Village Business Park to enhance the City's economic base, define new public spaces to serve the business park and the entire community, and create a model for sustainable, healthy development. The Specific Plan meets the City's goal of taking a long-term view of what land uses are appropriate for this area by providing greater flexibility in permitted land uses to capture economic potential both in the short-term and long-term. It provides for higher density zoning and the addition of housing at appropriate locations within the Specific Plan area, while not increasing density to the degree that it overburdens the surrounding street system and causes unavoidable significant impacts. It establishes a framework for viable, high quality development and public improvements that are in character with the quality of design established within the City and respect the high value the community places on open space.

B. Location and Setting

The City of Westlake Village is a suburban community located 38 miles west of downtown Los Angeles and nine miles from the Pacific Ocean. It is situated along the northwest border of Los Angeles County and is divided by US Highway 101. The City borders Ventura County and the City of Thousand Oaks to the north and west, the



City of Agoura Hills to the east, and the unincorporated Santa Monica Mountains to the south. Just over 50 percent of the City's total land area is open space, with nearly 75% of that land dedicated to preservation as open space. The developed portion of the City is primarily residential, with commercial developments along major arterials; industrial developments at the northern section; and public and institutional uses at



Figure 1-1:

Specific Plan Area

- Specific Plan Area
- City Boundary
- Focus Area

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



scattered locations. Several companies maintain their national or world headquarters in Westlake Village. The City encompasses twenty individual neighborhoods, with active homeowners' associations to promote and maintain high quality architectural standards.

The Specific Plan area (Figure 1-1) is located in the northern portion of the City and is approximately 200 gross acres in size (including public rights-of-way) and 183 net acres in size (excluding public rights-of-way)¹. It is bounded by Thousand Oaks Boulevard to the north, Lindero Canyon Road to the east, the Ventura Freeway (Highway 101) to the south and the City of Thousand Oaks to the west. The Specific Plan area contains 54 parcels with multiple property owners.

The focus of this Specific Plan is generally on the northern two-thirds of the planning area, which is the area in need of revitalization. The southern portion of the Specific Plan area contains the Four Seasons hotel, spa and wellness center, Dole corporate headquarters, Westlake Village Studios, Oaks Christian School, and Calvary Community Church, as noted in the previous section. The uses on these properties are intended to remain the same for the foreseeable future, along with several business park uses in the northern portion of the Specific Plan area. These properties are included within the Specific Plan area for context, as part of the larger business park, and because streetscape improvements are planned for the streets fronting these properties for continuity within the business park. No zoning changes are planned for these properties.

With the exception of the uses noted above in the southern portion of the Specific Plan area, existing land uses include a variety of business park and commercial land uses, including general office, light industrial, auto repair, distribution, and warehousing. Service uses occupy some of the multi-tenant space. The area is essentially built-out and the majority of parcels are less than two acres in size. The predominant building type is single story tilt-up industrial or office buildings.

Adjacent uses to the Specific Plan area include the Westlake Village Community Park/YMCA to the north of Thousand Oaks Boulevard; office development to the west; Costco and Valley Oaks Memorial Park Cemetery to the east of Lindero Canyon Road; and offices and the Shoppes at Westlake Village farther to the east along Russell Ranch Road.

1

Source: City of Westlake Village GIS data



In the southern portion of the Specific Plan area, large areas of land which formerly housed major industrial activities have been redeveloped with other uses which were deemed desirable by the City, such as the Four Seasons Hotel, Dole World Headquarters, Oaks Christian School, and Calvary Community Church. These uses improved the image of the business park, especially as viewed from the Ventura Highway (101 Freeway).



The northern portion of the Specific Plan area consists of varied office, service, light industrial, and warehousing uses.



C. Regulatory Authority

A Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a localized area. While the General Plan is the overall guide for growth and development in a community, a Specific Plan is able to focus on the unique characteristics of a special area by customizing the planning process and land use regulations to that area. The North Business Park Specific Plan is the second adopted specific plan in the City of Westlake Village, and therefore referred to as Specific Plan No. 2 (SP No. 2).

The North Business Park Specific Plan has been prepared and adopted by ordinance in compliance with the requirements of the California Government Code (Sections 65450 through 65507), and addresses all issues and topics specified therein. As such, the Specific Plan constitutes a comprehensive program for implementing applicable goals and policies of the Westlake Village General Plan.

The Specific Plan sets forth development standards, design guidelines and public realm improvements to ensure that the long-term development of the North Business Park occurs in a manner that is compatible with surrounding land uses and consistent with the high quality of design established within the City of Westlake Village.

D. Consistency with the General Plan, Zoning Ordinance and Strategic Plan

Per Section 65454 of the California Government Code, specific plans must be internally consistent with the jurisdiction's General Plan. The **Westlake Village General Plan** regulates land uses in the North Business Park Specific Plan area. As such, General Plan Figure 8, Westlake Village Land Use Plan, will be amended concurrent with the adoption of this Specific Plan to designate this area as SP No. 2. Similarly, the City's Zoning Map will be amended to identify this area as SP No. 2.

The land use entitlements and development standards set forth in this Specific Plan are consistent with the applicable goals and policies of the Westlake Village General Plan. Policies within the General Plan identify the area covered by this Specific Plan for a mixture of commercial, business park, high density residential, educational, and open space uses, consistent with this Specific Plan. All public works projects, tentative or parcel maps and zoning permits approved subsequent to the adoption of this Specific Plan shall meet the regulations, standards and criteria set forth herein.

The Specific Plan implements Community Development Goal 9 to "Promote the revitalization and more effective use of properties characterized by economic

underutilization or obsolescence through the implementation of a specific plan.” The Specific Plan is also consistent with the General Development Policy (Figure 7) of the General Plan, which identifies the North Business Park as an Intensification Area, and therefore anticipates and provides for recycling of uses and adaptive reuse of existing development at somewhat greater intensities.

The following General Plan Objective and Policies apply to this Specific Plan:

- Objective 9.3: Encourage the revitalization and reuse of the business park uses north of the Ventura Freeway and west of Lindero Canyon Road for the development of a mix of uses.
- Policy 9.3.1: Require that projects be designed to integrate development in a “village” character (i.e., cluster buildings on common walkways, open spaces, and plazas, incorporate facade articulation and vertical setbacks), and include extensive landscaping.
- Policy 9.3.2: Require the provision of on-site open space amenities designed to be accessible to and of sufficient size to be usable by tenants.
- Policy 9.3.3: Incorporate a range of uses spanning from residential to office to commercial, giving residents of Westlake Village and surrounding communities amenities consistent with ideals of a mixed use development.

The Specific Plan also facilitates and implements some of the key goals and objectives of the **City of Westlake Village 2025 Strategic Plan, including:**

- Goal 4: Promote economic growth and development.
- Objective: Assess aging business parks and/or commercial areas of the City for potential redevelopment opportunities.

- Goal 8: Undertake advance land use planning to ensure a continuation of a balance of land uses within the community.
- Objective: Focus on aging business park areas of the community in need of long-term redevelopment with uses consistent with the City’s General Plan.
- Strategy: Adopt and implement the North Business Park Specific Plan.

- Goal 10: Promote housing opportunities to accommodate all segments of the community including young adults, families and seniors.
- Objective: Where feasible, implement the Housing Element strategies contained in the City’s General Plan.
- Strategy: Adopt the North Business Park Specific Plan and facilitate various levels of housing options, including live/work districts.

E. Visioning and Community Participation

The North Business Park Specific Plan was prepared with extensive community outreach and participation. Input was obtained through a series of public workshops, where stakeholders including property owners, businesses, residents of adjacent neighborhoods, and interested parties provided input to the consultant team through group discussion and feedback on the topics and direction of the Plan. Study sessions were also held with the City Council, as well as with the Business Park Specific Plan Ad Hoc Committee, Land Use Committee, and Environmental Committee of the City Council in order to provide decision-maker continuity and feedback throughout the duration of the project.

Three public workshops/study sessions were held at milestone stages of the project: analysis and visioning, alternative land use scenarios, and preferred land use scenario. During the visioning process, the primary issues and ideas expressed by the stakeholders were focused on providing incentives for upgrades and new development, improving parking and amenities for the business park, and being receptive to future market changes and the possibility of major change in the long term. Stakeholders discussed their vision to transform the business park into a vibrant area with a mix of uses, while simultaneously guiding future growth in a way that is compatible with surrounding development and the existing characteristics of this master planned community. In addition, several goals for the North Business Park Specific Plan were developed as a result of the input received:

- Increase city revenues;
- Support property owners and tenants;
- Maintain and enhance community image;
- Improve circulation;
- Enhance open space;
- Provide services for the community;
- Provide housing opportunities;
- Reduce vehicle miles traveled;
- Accommodate growth; and
- Become more of a destination.

The next phase of the project was to prepare conceptual alternative land use scenarios to discuss with the stakeholders. The majority of the stakeholders favored the idea of allowing additional land uses in the area to provide for greater market flexibility in the future, as well as increasing density potential. The stakeholders envisioned long-term potential for making the Specific Plan area a “creative”, “campus” environment; a “village” way of living in a live/work environment. The stakeholders were also in favor of a creating

a Business Improvement District (BID) for the area to help incentivize the public improvements, promote the area, and generate more pride and a sense of ownership among the property owners.

As a result of input received on the alternative land use scenarios, a preferred land use scenario and urban design concept was prepared and discussed in meetings with the Ad Hoc, Land Use, and Environmental Subcommittees of the City Council. In addition, the preferred scenario was presented, and comments received, at a public City Council study session. In keeping with the comments received at the previous workshops, the Committees and Council members expressed a desire to allow for a broader range of uses and flexibility for significant change over the long-term to avoid furthering obsolete uses and under-utilized sites within the business park. An overriding short-term suggestion for Specific Plan implementation was to phase the project by first investing in public improvements in the Design District along La Baya Drive to “kick start” redevelopment of the area. Public investments in this area can be fairly minor to start, including signage and street improvements, but have the potential to create identity and generate private investment in the Specific Plan area.



Stakeholders and community members provided extensive input to the future of the North Business Park at several public workshops and study sessions held throughout the Specific Plan process.

This input which was received throughout the community engagement process resulted in a Draft Specific Plan that was completed in 2012. As described earlier in this chapter, the 2012 Draft Specific Plan was never adopted due to projected traffic increases that could not be fully mitigated and were above the community’s acceptance level, as well as changes in economic and physical conditions which lowered the market demand for some of the retail uses envisioned for the Specific Plan area. As a result, the City revisited the land use assumptions for future development and conducted further traffic and economic studies to achieve a build out scenario for the Specific Plan area that better balances economic feasibility with the goal of having acceptable intersection levels of service on the surrounding street system. This Specific Plan is a result of those efforts, while maintaining the goals and vision set forth by the community.

Goals and Policies

A. Introduction

The Specific Plan goals and policies support the transition from a suburban business park to an area with a mix of uses to revitalize the aging business park and to ensure a continuation of a balance of land uses within the community. To maximize the redevelopment potential of the area, the Specific Plan recommends improving traffic circulation, introducing complimentary commercial and residential uses, and enhancing pedestrian amenities. The goals and policies acknowledge that private investment will lead the area's transition and recommend several different public/private partnership strategies that will increase the competitiveness and development potential of the Specific Plan area.

The goals and policies set forth the framework to realize the vision for the North Business Park Specific Plan. They serve as guidelines for decision making and provide direction for the future. The goals and policies were derived from input received from the community and stakeholders at public workshops, City Council study sessions, and City staff during the planning process. The goals and policies are provided for under the following categories:

- Land Use and Urban Design
- Economic Development
- Circulation
- Parking
- Infrastructure

The Specific Plan goals and policies are consistent with the citywide policies of the General Plan, some of which are fully or partially incorporated and referenced herein. In addition, and as described in Chapter 1, this Specific Plan supports policy direction provided by the Westlake Village General Plan by facilitating intensification in areas that are economically underutilized and a mix of land uses to provide flexibility and better respond to market demand.

B. Land Use and Urban Design

Goal LU/UD-1: Provide for development within the Specific Plan area by designating appropriate land uses and intensities to meet the needs of anticipated growth and to achieve the community's objectives.

Policy LU/UD-1.1: Provide for intensification at appropriate locations provided that the proposed use is compatible in use, scale and density with adjacent uses and further provided that the proposed use is compatible with existing or planned infrastructure capacity and availability.

Policy LU/UD-1.2: Establish land use districts that have complimentary rather than competitive uses and maintain the integrity of, and interrelationships among, the districts.

Policy LU/UD-1.3: Accommodate employment, service and residential uses, as well as local and regional-serving amenities within a comprehensive mixed use environment.

Goal LU/UD-2: Respond to market trends, developer interest and community objectives by creating a forward-looking and responsive land use plan for the Specific Plan area.

Policy LU/UD-2.1: Diversify the mix of land uses to respond to market demand, create a vibrant and more active environment, make the most efficient use of available land, and ensure a balance of land uses within the community.

Policy LU/UD-2.2: Facilitate the development of larger-scaled unified projects, rather than piecemeal development, by incentivizing the consolidation of parcels where appropriate.

Policy LU/UD-2.3: Identify site opportunities and actively recruit developers of projects that integrate compatible uses and pedestrian amenities.

Policy LU/UD-2.4: Take advantage of the Specific Plan area's prominent location and accessibility along US Highway 101 by encouraging land uses with a regional draw, in addition to serving the local community.

Policy LU/UD-2.5: Assist in the long-term redevelopment of auto-related uses (repair, sales, body work, parts, car wash, etc.) in the Specific Plan to provide opportunities for commercial uses consistent with the intent of the Design District.

Policy LU/UD-2.6: Consider prioritizing investment in public improvements (streetscape improvements, signage, banners, etc.) along La Brea Drive in the Design District to “kick start” redevelopment of the area.

Goal LU/UD-3: Create a range of housing opportunities and choices.

Policy LU/UD-3.1: Implement targeted areas of mixed use zoning that promotes employment uses proximate to housing.

Policy LU/UD-3.2: Introduce higher density housing products to facilitate various levels of housing options, including live/work districts, and to offer housing choices that are less available in Westlake Village.

Goal LU/UD-4: Create a vibrant environment for both residents and visitors.

Policy LU/UD-4.1: Incorporate a range of uses spanning from residential to office to commercial, giving residents and surrounding communities amenities consistent with a mixed use, “village” environment.

Policy LU/UD-4.2: Require the provision of on-site open space amenities designed to be accessible to and of sufficient size to be usable by residents, tenants, and visitors.

Goal LU/UD-5: Encourage good design and high-quality development within the Specific Plan area.

Policy LU/UD-5.1: Implement development and design standards that result in high quality development of distinctive character.

Policy LU/UD-5.2: Require that projects be designed to integrate development in a “village” character (i.e., cluster buildings on common walkways, open spaces, and plazas, incorporate façade articulation and vertical setbacks), and include extensive landscaping.

Policy LU/UD-5.3: Take advantage of the natural setting and dramatic views of the Santa Monica Mountains to enhance the quality of the overall development.

Policy LU/UD-5.4: Implement development standards and design guidelines to provide an appropriate transition between commercial uses and adjacent residential uses.

Goal LU/UD-6: Encourage sustainable design and development practices.

Policy LU/UD-6.1: Encourage efficient patterns of development within the Specific Plan area by facilitating mixed use development that maximizes pedestrian connectivity and minimizes the need for vehicle travel.

Policy LU/UD-6.2: Encourage design that takes advantage of the area's natural resources, such as topography, wind, sun, etc., and emphasize environmental sensitivity and sustainable development practices throughout the Specific Plan area.

Policy LU/UD-6.3: Implement standards and guidelines for sustainable development based on best management practices and available and emerging technologies in the design, construction and long-term maintenance of projects.

Policy LU/UD-6.4: Through the development process, encourage building orientations conducive to utilizing available solar energy.

Policy LU/UD-6.5: Encourage projects to achieve the Leadership in Energy and Environmental Design (LEED) Certification or other similar certification.

Policy LU/UD-6.6: Require new development to incorporate amenities to encourage bicycling, including bicycle racks, lockers, and bicycle paths between uses where feasible.

Goal LU/UD-7: Enhance the pedestrian environment and provide for comfortable settings in which people can gather.

Policy LU/UD-7.1: Create pedestrian linkages between districts in the Specific Plan area, as well with the Westlake Village Community Park to the north of Thousand Oaks Boulevard.

Policy LU/UD-7.2: Improve the pedestrian environment along all streets within the Specific Plan area with sidewalks and streetscape enhancements, such as street trees and street furniture.

Policy LU/UD-7.3: Locate streetscape elements to enhance the public realm by framing views, screening parking areas, identifying entries, providing shade, etc.

Policy LU/UD-7.4: Provide for the creation of gathering places within private development, such as plazas, green spaces, and linear parks that capture views.

Policy LU/UD-7.5: Provide site furniture and lighting appropriate to the “village” environment and unique to the Specific Plan area.

C. Economic Development

Goal ED-1: Provide for adequate infrastructure financing for existing and future development.

Policy ED-1.1: Require existing and new development to contribute their fair share of the cost of on- and off-site public infrastructure.

Policy ED-1.2: Prioritize public investment that improves traffic circulation and expands streetscape improvements to needed areas.

Policy ED-1.3: Consider innovative financing mechanisms, including, but not limited to, establishing Community Facilities Districts (CFDs), Special Assessment Districts, Enhanced Infrastructure Financing Districts (EIFD), Development Impact Fees and participation in City’s Capital Improvement Program (CIP) to fund and construct necessary public facilities and infrastructure.

Policy ED-1.4: Based on capital cost estimates provided as part of the Specific Plan, establish development impact fees for new development’s fair share cost of required facilities.

Policy ED-1.5: Apply for available State, Federal and regional funding sources to finance infrastructure costs.

Policy ED-1.6: Periodically update the financing plan as modifications to financing programs, land uses, and cost estimates for infrastructure and public facilities occur.

Goal ED-2: Provide for adequate coverage of operations and maintenance costs for existing and future development to achieve a fiscally sound plan.

Policy ED-2.1: Require existing and new development to contribute their fair share of operations and maintenance costs for enhanced services that provide special benefits to properties and businesses.

Policy ED-2.2: Use various combinations of techniques to cover ongoing operations and maintenance costs, such as landscape and lighting districts or Community Facilities District (CFD) special taxes.

Policy ED-2.3: Provide for approaches that increase the cost-efficiency of the delivery of public services.

Goal ED-3: Diversify and increase City revenues that lead to a more fiscally balanced community.

Policy ED-3.1: Facilitate efforts to increase the sales tax revenues from such activities as retail development that serves the business park and community residents, such as: convenience retail, specialty retail, restaurants and food establishments.

Policy ED-3.2: Facilitate efforts to expand the presence of businesses that constitute a design district that also generate taxable sales.

Policy ED-3.3: Build upon city programs, such as improving transit access and parking management that can lead to increased development and enhanced property values.

Policy ED-3.4: Coordinate with property owners and businesses in marketing efforts that help establish a strong sub-regional presence as specialty retail and design district destination.

Policy ED-3.5: Encourage residential development that is compatible with commercial uses and can support community serving businesses.

Policy ED-3.6: Prepare a set of performance indicators to monitor ongoing fiscal health of the business park.

Goal ED-4: Provide incentives for future development to assemble and make efficient utilization of land.

Policy ED-4.1: Target city programs that can provide financial assistance for the provision of a range of low to moderate-income housing types and prices/rents.

Policy ED-4.2: Target city programs that increase the transit accessibility between the business park and the entire community.

Policy ED-4.3: Implement parking management public/private partnerships solutions that can accommodate both parking needs of new development as well as broader community shared parking needs.

Policy ED-4.4: Provide for a reimbursement program to developers/land owners if they finance public infrastructure beyond their fair share as development impact fee revenues are collected.

Goal ED-5: Facilitate public/private partnerships that allow the private sector to increase their competitiveness and guide the future of their development.

Policy ED-5.1: Within the commercial and business/industrial areas, encourage the formation of a property-based Business Improvement District (BID) to provide enhanced services, such as marketing, beautification, signage and property owner coordination and representation.

Policy ED-5.2: Include representation in the BID of the residential development in order to create dynamic and compatible mixed use development.

Policy ED-5.3: Work with County and regional agencies that can provide financial assistance for the development of locally and regionally integrated transportation systems that encourage improved jobs-housing balance.

Policy ED-5.4: Explore the feasibility of a parking district featuring off-site structured parking. Where appropriate, utilize parking districts to encourage more efficient use of existing properties and encourage property assemblage for higher density uses.

Policy ED-5.5: Continue to coordinate with neighboring jurisdictions in efforts that increase the economic development competitiveness of the sub-region.

D. Circulation

Goal C-1: Improve the circulation system within the Specific Plan area by maintaining and improving the roadway system, providing for convenient access to, and circulation within, the Specific Plan area for all modes of transportation and, in particular, enhance walkability and connectivity in the area.

Policy C-1.1: Maintain consistency, where possible, with the provisions and policies for all transportation modes as discussed in the City of Westlake Village General Plan Circulation section.

Policy C-1.2: Implement the Complete Streets¹ concept when considering improvements to the local street system.

Policy C-1.3: Improve pedestrian circulation throughout the Specific Plan to create an

¹ Complete Streets is a national movement, which has been endorsed and adopted by the State of California, to ensure transportation planners and engineers consistently design and operate the entire right-of-way with all users in mind—including motorists, bicyclists, public transportation users, and pedestrians of all ages and abilities.

environment where people can walk to various activity points within the Specific Plan area and connect with adjacent areas, allow people to accomplish local trips without driving, and to contribute towards a human-scale and sense of community.

Policy C-1.4: Support the development of infrastructure implementation strategies focused on encouraging the use of electric and other non-carbon emitting vehicles.

E. Parking

Goal P-1: Provide a sufficient supply of parking within the Specific Plan area to meet future demand with build-out of the area without providing unneeded parking that wastes space and money.

Policy P-1.1: Encourage the use of shared parking facilities wherever possible, both in mixed-use districts and among specific uses with recognized different peak demand times and parking demand pattern over time.

Policy P-1.2: Consider the establishment and operation of a parking district for the Specific Plan area or districts within the Specific Plan area. Joint development of shared public parking facilities via a parking district may allow better shared use of parking spaces than does provision of on-site private parking.

Policy P-1.3: Provide bicycle parking for employees, residents and patrons who bicycle to, from, and within the Specific Plan area in such a way as to be attractive, safe, convenient, and to encourage bicycling as a transportation mode.

F. Infrastructure

Goal I-1: Provide fully functional, safe, cost-effective and environmentally-friendly public infrastructure to meet the needs of future development within the North Business Park Specific Plan area.

Policy I-1.1: Continue to coordinate with and fully utilize the resources of the various coordinating agencies to provide sufficient levels of water, sewer, and storm drain service throughout the Specific Plan area.

Policy I-1.2: Continue to coordinate with the dry utility service providers to ensure adequate provision of electricity, natural gas, telephone and data services to the Specific Plan area.

Goal I-2: Ensure that an adequate infrastructure system is in place for future residents and businesses in the Specific Plan area.

Policy I-2.1: As a condition of development approval, ensure that utilities are adequately sized to accommodate the proposed development and, if applicable, sized for other future developments.

Policy I-2.2: Require individual projects to provide comprehensive infrastructure plans for City review and approval as part of a development application.

Goal I-3: Provide environmentally efficient and sustainable infrastructure improvements.

Policy I-3.1: Enforce the State of California Low Impact Development (LID) practices for all new development, which will provide for conservation of natural features and reduce long-term maintenance and life cycle costs.

Policy I-3.2: Strongly encourage the provision of “green” infrastructure, such as “green” streets, solar panels, heat reflective roofs, green roofs, etc. to minimize environmental impacts of development.

Policy I-3.3: Require the expanded use of recycled wastewater for irrigation, dust control, soil compaction, fire protection, and other uses as they are developed, as a means of reducing impacts on ground water resources.

Goal I-4: Minimize the impacts of new utilities on view corridors and the natural and built environment.

Policy I-4.1: Require undergrounding of new utility lines, with priority given to the undergrounding of utility lines along major streets. This will also allow trees to reach full height and improve the aesthetics of the area.

Specific Plan Administration

A. Introduction

This chapter describes the procedures required for the timely implementation of development within the North Business Park Specific Plan area. Several documents have been prepared and processed concurrently with the adoption of this Specific Plan which include a general plan amendment, zoning map amendments, and certification of an Environmental Impact Report (EIR). These documents will form the basic framework to guide future development within the Specific Plan area.

B. Applicability

Adoption of this Specific Plan applies zoning designations for the Specific Plan area as set forth in Chapter 4, which incorporates standards for land use and development. No construction, modification, addition, or placement of any building or structure shall occur, nor shall any new use commence on any lot, on or after the effective date of this Specific Plan that is not in conformity with the provisions of this Specific Plan. Unless otherwise provided by specific provisions of this Specific Plan, permitted land uses and activities within the area covered by this Specific Plan shall comply with all applicable requirements of Article 9 of the Westlake Village Municipal Code (Planning and Zoning Regulations).

The provisions of the Specific Plan shall not apply to development projects for which an entitlement has been approved by the City and vested right has been obtained before the effective date of the Specific Plan, although applicants for such projects have the option to comply with the provisions of the Specific Plan upon adoption.

Deviations from the standards contained in this Specific Plan may be approved by the City Council if it is determined that:

1. Imposition of one or more of the applicable development standards set forth herein would prevent or substantially frustrate achievement of development entitlements otherwise authorized by this Specific Plan; and

2. Modified standards, imposed as conditions of approval of a Planned Development Permit, will achieve a high quality of project design consistent with the intent of this Specific Plan.

C. Planned Development Permit

Except as noted herein, all construction projects in the North Business Park Specific Plan area shall file an application for a Planned Development Permit pursuant to Chapter 9.25 of Article 9 and shall be subject to the site/design review process set forth in Chapter 9.25. The Planned Development Permit process is intended to protect the integrity and character of the residential, commercial and industrial areas of the City through the application of the provisions in Chapter 9.25, consistent with the objectives, policies, land uses and programs of the Westlake Village General Plan.

A Planned Development Permit shall be required under the following circumstances for property within the North Business Park Specific Plan area:

1. For a new structure.
2. For significant enlargements or significant exterior alterations of an existing structure for which a planned development permit has never been issued.
3. For any change in use of a structure or a parcel of land for which a planned development permit has never been issued.
4. For temporary structures, in conjunction with existing, permanent facilities and occupying the same lot or parcel as said permanent facilities.

A change of use which is similar in nature to a previously permitted use or an exterior alteration which maintains the same architectural character or theme and minor additions not to exceed five hundred (500) square feet which maintain the same architectural character or theme may be allowed through the issuance of a zone clearance, subject to the approval of the Planning Director and City Manager.

D. Design Review

All new development and improvements greater than 50% of the existing building square footage will be subject to professional Design Review by the City at the cost of the applicant. In addition, all new development projects and substantial landscape improvements will be subject to professional Landscape Design Review by the City at the cost of the applicant. The Design Guidelines in Chapter 5 of this Specific Plan shall apply, as well as Chapter 9.15 (Design Standards) and Chapter 9.16 (Landscape Design Standards) of the Zoning Code.

E. Signage

Signage in the North Business Park shall meet the permit, processing and regulations for signs contained in Chapter 9.18 (Signs) of the Zoning Code. In multi-tenant developments, no sign shall be displayed at a commercial, industrial, or public/institutional complex until the property owner has received City Council approval of a sign program for the complex that meets the sign allotment regulations for a multi-unit complex per Chapter 9.18 (Signs).

F. Application of the CEQA Process

The Environmental Impact Report (EIR) for the North Business Park Specific Plan provides the required environmental documentation per Section 15168 of the California Environmental Quality Act (CEQA) Guidelines. The EIR is structured as a program-level EIR for the entire Specific Plan area. Hence, individual development projects must be examined in light of the Final Program EIR to determine whether additional environmental documentation must be prepared. If the specific project is found to have no new impacts and no more intense impacts than those disclosed in the Final EIR, the specific project may be approved using the Final EIR. Otherwise, subsequent environmental documentation may be necessary.

1. Specific Plan Buildout

The development capacity of the Specific Plan area (or planning area) is based upon assumed levels of development for the different land use districts. The City does not expect that all properties within each district of the planning area would be redeveloped at the maximum densities and intensities as allowed by the Specific Plan. For this reason, the Specific Plan and Program EIR differentiate between “theoretical buildout” and “evaluated maximum buildout” of the Specific Plan.

Theoretical Buildout

Theoretical buildout of the Specific Plan would occur if all parcels within the planning area are developed to the maximum densities and intensities using the maximum floor area ratios and highest residential or commercial densities allowed under the proposed Specific Plan. The theoretical buildout is useful to articulate on a parcel by parcel basis because it is likely that some property owners will try to build out to the maximum capacity, but it is highly unlikely that all properties within the Specific Plan area would develop in this manner.

Site constraints (e.g., lot shape, slope, easements, and access), development standards (e.g., required setbacks, lot coverage, height limits, and landscaping and parking requirements), and design considerations would generally prevent development at maximum density/intensity on every lot. Additionally, some properties are not currently developed to the maximum allowed under the General Plan and Zoning Code and may not be redeveloped. For one, the Specific Plan assumes that a number of existing developments at the southern portion of the planning area would be preserved or maintained in their current state, along with some business parks in the Focus Area.

Assuming the complete redevelopment of existing properties would not be a realistic or necessarily a profitable development scenario for many properties, especially for relatively new developments that might only gain an incremental increase in site density.

Because the City does not expect the theoretical buildout potential to be achieved at any time due to various factors, including physical constraints, policy constraints, and economic realities, an assessment of the theoretical buildout of the planning area is not useful. Thus, the Program EIR does not evaluate the environmental impacts of theoretical buildout. Instead, it evaluates the impacts of the evaluated maximum buildout of the planning area, as set forth in the Specific Plan.

Evaluated Maximum Buildout

Since the planning area is developed, the evaluated maximum buildout represents new development that would replace existing land uses and introduce new residential and commercial land uses. The North Business Park Specific Plan sets forth policies that are intended to preserve existing developments at the southern portion of the planning area, with redevelopment of existing land uses in the northern portion (Focus Area). A number of business parks in the Focus Area are also considered stable and expected to remain at current intensity and activity levels. Since the land uses that are expected to remain are not developed at maximum densities and intensities, theoretical buildout would not likely occur.

To account for a more realistic future development scenario, the Economic Analysis for the Specific Plan includes several assumptions for determining an appropriate maximum buildout for the planning area. The analysis accounts for employment and demographic trends, real estate development trends, market and fiscal assumptions, and pro forma analyses.

As indicated earlier, existing land uses on the southern portion are expected to remain in place; no additional development is anticipated in this area. Future development in the Focus Area assumes that new dwelling units would be developed as part of

mixed use developments or on specific parcels in mixed use districts at a maximum allowable density of 35 units per acre. However, physical conditions and Specific Plan requirements for on-site improvements would generally limit the ability of parcels in the mixed use districts to achieve maximum densities. Also, the projected resident population assumes an average of 2.25 persons per household, which is lower than the Citywide average of 2.54 persons per household. This smaller household size takes into account the type (e.g., attached multi-family units and condominiums) and smaller size of dwelling units anticipated to be developed in the Focus Area compared to the City's predominantly single-family detached units.

For the mixed-use districts, the assumed ratio of residential to non-residential development is 60 to 40 percent, with approximately 60 percent of the total area developed with residential uses and approximately 40 percent of the total area developed with non-residential uses. Non-residential development would have maximum allowable intensities (i.e., floor area ratios [FAR]) of 0.5 but the maximum buildout projections represent a smaller developable land area for each district and a lower FAR, given current development intensities.

With these assumptions, the evaluated maximum buildout in the Focus Area of the Specific Plan includes:

- 1,017 new dwelling units
- 6,780 square feet of restaurant uses
- 425,790 square feet of office uses
- 215,045 square feet of specialty retail uses
- 840,601 square feet of flex space/business park uses
- 59,240 square feet of other service uses
- 83,936 square feet of school-related ancillary uses

2. Monitoring and Implementing Maximum Buildout

To ensure that development does not exceed the maximum buildout evaluated in the Program EIR, the City would have to monitor and track development within each district in the Focus Area. Should a property owner or the market warrant development in excess of what is assumed in the evaluated maximum buildout, additional environmental review under the California Environmental Quality Act (CEQA) may be required prior to the approval of a site-specific project that would result in the additional residential units or non-residential development. In addition, to avoid any new significant traffic-related impacts, development within each district would also be required to show that it would not exceed the projected AM/PM peak hour in/out volumes for each district, even for an interim period of time. This trip generation is presented in Table 3-1.

As part of its development monitoring and tracking efforts, the City would require daily and peak hour trip generation estimates from each new development. Should proposed development exceed the traffic caps for its district, as provided in Table 3-1, the project applicant would have to prepare a detailed traffic study analyzing the existing traffic conditions, the impacts of the proposed project on the surrounding roadway network, and mitigation measures necessary to mitigate impacts that exceed applicable City standards.

Table 3-1: Specific Plan Trip Generation

Specific Plan Zoning Districts	Daily Trip Ends ¹ Volumes ²	AM Peak Hour Volumes			PM Peak Hour Volumes		
		In	Out	Total	In	Out	Total
Mixed Use Corsa	2,488	104	75	179	106	129	235
Mixed Use Lindero	4,049	165	162	327	165	204	369
Office District	2,240	230	37	267	42	223	265
Design District (North)	6,814	370	89	459	258	431	689
Design District (South)	6,203	96	58	154	301	325	626
Mixed Use Cedarvalley	2,551	244	43	287	67	191	258
Business Park (West)	3,011	288	51	339	79	226	305
Business Park (East)	1,612	154	27	181	42	121	163
Total	28,968	1,651	542	2,193	1,060	1,850	2,910

Table 3-1 Notes:

¹Trips are one-way traffic movements, entering or leaving.

²Minor reallocation of traffic volumes may be permitted between zoning districts provided that total allowable volumes for the Specific Plan area are not increased.

3. Scope and Future Use of Program EIR

The Program EIR is intended to serve as the primary environmental document for all future entitlements (later activities) associated with implementation of the North Business Park Specific Plan, including all discretionary approvals requested or required to implement the Specific Plan. Thus, development beyond the evaluated maximum buildout of the planning area would require subsequent environmental review, as the Program EIR only evaluates the impacts associated with the evaluated maximum buildout, as contained in the proposed Specific Plan.

Pursuant to Section 15168 of the CEQA Guidelines, a later activity under the proposed Specific Plan must be examined in the light of the Final Program EIR to determine whether additional environmental documentation must be prepared. Each later activity

must undergo an analysis by the City to determine if the activity is within the scope of the Final Program EIR. Because these later activities may have been previously analyzed in the Program EIR, compliance for each impact category is narrowed to a determination as to whether the activity would result in: (1) no substantial change to the anticipated impacts discussed in the previous analysis; (2) a more severe impact; or (3) a new significant impact. This would include review of project compliance with the maximum dwelling unit and/or non-residential floor area buildout and the trip generation caps for each land use district.

Based on the results of the analysis for each development proposal or project, the City would determine which of the following actions is applicable to the later activity:

- a) The later activity is a component of and consistent with the North Business Park Specific Plan and has been previously analyzed as a part of the Final Program EIR and the findings certified pursuant to the State CEQA Guidelines. No additional CEQA documentation is required (CEQA Guidelines Section 15168).
- b) The later activity is a component of the North Business Park Specific Plan and has been previously analyzed as a part of the Final Program EIR and the findings certified pursuant to the State CEQA Guidelines; however, minor technical changes or additions are needed to make the previous documentation adequate to cover the project. An Addendum to the Final Program EIR is required (CEQA Guidelines Section 15164).
- c) The later activity is either not a component of the North Business Park Specific Plan; has not been previously analyzed as part of the Final Program EIR; represents a substantial change to the Specific Plan or the circumstances under which the project is undertaken; or information of substantial importance has become known, in which case an Initial Study and additional environmental review under CEQA would be required (CEQA Guidelines Section 15162), unless the later activity is determined to be exempt under CEQA.

4. Mitigation Reporting and Monitoring Program

Pursuant to Section 21081.6 of the California Environmental Quality Act (Public Resources Code) and Section 15097 of the State CEQA Guidelines (California Code of Regulations), public agencies are required to adopt a monitoring and reporting program to assure that the mitigation measures identified in the Final Environmental Impact Report (FEIR) for a project are implemented.

The Mitigation Reporting and Monitoring Program (MMRP) is intended to provide reasonable assurance that each development project under the North Business Park Specific Plan shall be in force and effect at all times during implementation of the development projects contemplated within the North Business Park Specific Plan. A Mitigation Monitoring and Reporting Program Checklist is included in the Final EIR for the North Business Park Specific Plan. The City shall use the checklist to identify the mitigation measures in the Final EIR that are applicable to a proposed development, as part of project approval.

The applicant for each development project shall prepare and submit to the City written status reports demonstrating good faith compliance with those conditions of project approval which have been identified as applicable mitigation measures. These reports shall be submitted to the Planning Director semi-annually. The Planning Director shall specify more frequent reporting intervals when warranted based upon any of the following situations:

- a) The project is in an active development phase, or
- b) Seasonal environmental conditions require more frequent reporting, or
- c) Specific project activities, such as grading or Oak Tree removal and relocation, require more frequent reporting, or
- d) The Planning Director has reason to believe there is threatened or actual non-compliance with mitigation measures, or
- e) The City Council, in the exercise of its judgment, determines that more frequent reporting is necessary or desirable.

The Planning Director or his or her designee shall independently review and take reasonable steps to verify the applicant's efforts to comply with identified mitigation measures. Such actions shall include, at a minimum, arranging and conducting site inspections by public officials with jurisdiction over the mitigation measure and/or impact, preparation of written reports or memoranda as needed to document mitigation measure compliance, and preparing periodic compliance reports to the City Council. In the event the City determines, based upon substantial evidence, that there has been non-compliance with any adopted mitigation measure, it shall notify the applicant of the situation and direct that immediate steps be taken to cure the non-compliance. In the event the applicant fails to comply with such notice, the City shall take all available legal steps to compel the applicant to comply with the adopted mitigation measures, including, but not limited to, actions to enforce building code provisions, actions to revoke or suspend permits, and referral of the matter to the Attorney General and District Attorney, if appropriate.

G. Specific Plan Amendments

The City Council has the authority to amend the Specific Plan subject to the following:

1. Amendments to the Specific Plan are consistent with the goals, policies, and programs of the General Plan; and
2. Approval of the amendment will not be detrimental to the public interest, safety, health or general welfare, and will not be detrimental or injurious to property or improvements located adjacent to, or within the same vicinity as the property for which said modification is granted.

Specific Plan Zoning

A. Introduction

This chapter establishes zoning districts for the North Business Park Specific Plan and sets forth use regulations and development standards. The requirements of this chapter, in conjunction with the Design Guidelines in Chapter 5, are intended to provide development and design criteria that reinforce the desired character for the North Business Park Specific Plan area.

The Specific Plan zoning districts are intended to facilitate revitalization of the North Business Park into a high quality mixed use environment by permitting a combination of business park, commercial, office, service, and residential uses within the Specific Plan area. The Specific Plan allows for a broader range of uses to provide flexibility for change over the long-term to avoid furthering obsolete uses and underutilized sites within the business park. The permitted uses in each of the Specific Plan zoning districts vary in composition and intensity based on location, accessibility and surrounding context. The zoning districts provide for moderate intensification and the addition of higher density housing at appropriate locations within the Specific Plan area to capitalize on the demand for more housing choices in the community. Also, the addition of residents in the area will help stimulate demand for ancillary retail, restaurants and cafes, and personal services in the Specific Plan area.

The intent of the mixed use approach to several of the Specific Plan zoning districts is to provide for a combination of land uses within the boundaries of the Specific Plan area, not necessarily a combination of uses on individual lots. A combination of uses may not be feasible or appropriate on an individual lot based upon the unique characteristics of the lot, including the lot's size, shape, location, existing development on the lot, and/or the lot's surrounding uses and development. When feasible on individual lots, a mixed development may integrate uses vertically within the same structure, or horizontally, with uses side by side on the same parcel.

All new development within the North Business Park shall be designed to promote the ideals of a mixed use environment, with high quality amenities, and, when feasible, shared access and parking with adjacent development. Property owners are encouraged to consolidate properties and/or to work together to create unified development. New development shall be constructed in a manner that provides interaction between adjacent uses and emphasizes the pedestrian environment.

B. Establishment of Specific Plan Zoning Districts

The North Business Park Specific Plan establishes zoning for parcels within the Specific Plan boundary as follows and as identified in Figure 4-1:

- Mixed Use Corsa
- Mixed Use Lindero
- Design District
- Office
- Mixed Use Cedarvalley
- Business Park (BP)
- Public/Institutional (PI)
- Commercial Planned Development (CPD)

The Commercial Planned Development (CPD), Business Park (BP), and Public/Institutional (PI) zones are existing zoning designations within Article 9 of the Westlake Village Municipal Code (Planning and Zoning Regulations). Refer to the appropriate chapters of Article 9 for use regulations and development standards that apply to these zones:

- Commercial Planned Development (CPD) – Chapter 9.8
- Business Park (BP) – Chapter 9.10
- Public/Institutional (PI) – Chapter 9.12

The Mixed Use Corsa, Mixed Use Lindero, Mixed Use Cedarvalley, Office, and Design Districts are regulated within this chapter and their intended character is described below. Chapter 5 provides design guidelines for all buildings within the Specific Plan area, and Chapters 6 and 7 set forth public realm improvements.

1. **Mixed Use Corsa.** The Mixed Use Corsa zoning district provides opportunities for residential, office, and restaurant uses in a pedestrian-oriented environment. Ancillary personal and convenience services and business support services are also permitted. Such development is intended to facilitate the grouping of innovative housing options with employment uses, public gathering spaces, and community amenities. This zoning district is intended to contain a mixture of residential and office uses and is not intended

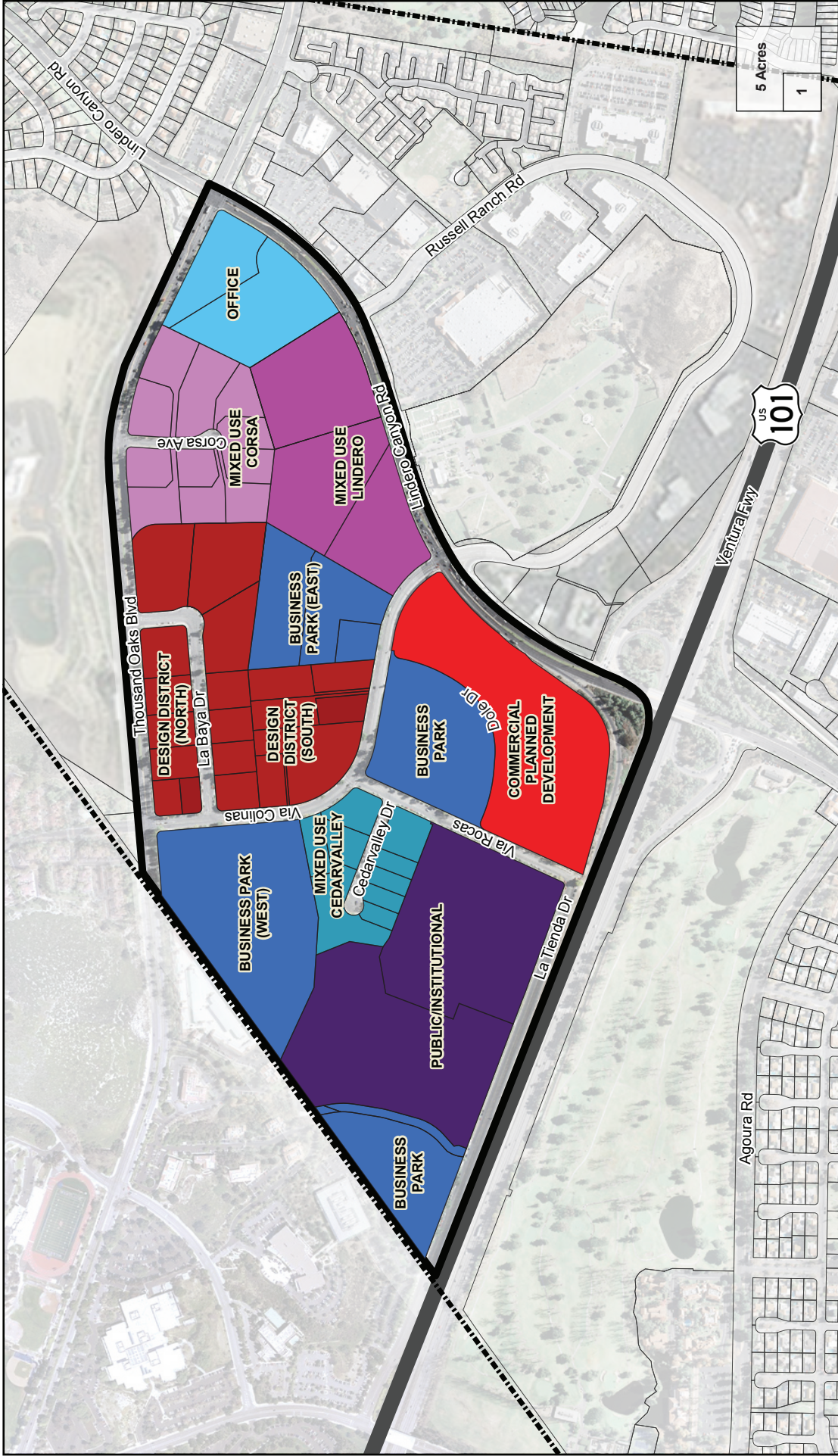
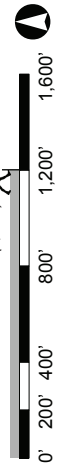


Figure 4-1:

Specific Plan Zoning

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



- Specific Plan Area
- City Boundary
- Mixed Use Corsa District
- Mixed Use Lindero District
- Design District
- Office District
- Mixed Use Cedarvalley District
- Business Park (BP)
- Public/Institutional (PI)
- Commercial Planned Development (CPD)

to become entirely residential or solely commercial in nature. The Mixed Use Corsa zoning district fosters pedestrian-oriented activity by providing a mix of uses in a compact and walkable area and encouraging large areas of open space in a plaza or village green setting. Attached residential uses are permitted within a horizontal or vertical mixed use setting.

The topography of this district is striking, with outstanding views of the City and Santa Monica Mountains to the south. The views from this district are an important asset that should be captured. Public open spaces, outdoor dining, and residential uses should be located on the southern portion of the site, where feasible, to take advantage of the scenic views. Standards for new development include the provision of open space along the ridgeline in the form of a linear park to take advantage of the southerly views. New residential uses will be able to take advantage of the proximity to the Westlake Village Community Park located directly north across Thousand Oaks Boulevard.

2. **Mixed Use Lindero.** The Mixed Use Lindero zoning district is intended to provide for corporate office and attached residential uses. This district allows for office uses to maintain this area as a major employment center. In addition, market forces are already suggesting transition of some office uses to attached residential units along Lindero Canyon Road. The visibility and accessibility of residential units along Lindero Canyon Road is beneficial with adjacent office uses to maintain a jobs-housing balance.
3. **Office.** The Office zoning district provides opportunities for increased general office uses and is located at the southwest corner of Thousand Oaks Boulevard and Lindero Canyon Road, a prime intersection in the City for professional and corporate office uses. It is also the location of the Guitar Center corporate headquarters. The Office District is intended to continue to be a major employment center to support the residential uses in the Specific Plan area.
4. **Mixed Use Cedarvalley.** The intent of the Mixed Use Cedarvalley District is to provide for existing office and business park activities, as well as accommodate the transition of several buildings to ministry and educational support uses, including student housing, administrative and classrooms, as part of the Oaks Christian School and Calvary Community Church campus. Parking for these uses will be provided on the Oaks Christian School/Calvary Community Church campus and pedestrian connections will be made to the adjacent campus.
5. **Design District.** Building upon what is already occurring in this area, the Design District zoning designation provides for the existing and future expansion of commercial, retail and service uses, with a focus on design and home improvement



The Mixed Use Corsa and Mixed Use Lindero districts could accommodate a variety of uses such as office, residential, restaurants and support service uses. Moderate intensification of these districts will continue to allow for high quality office uses, while also allowing for a transition to higher density residential and mixed use development. Such development is intended to facilitate the grouping of innovative housing options with employment uses, public gathering spaces, and community amenities.

products, especially along La Brea Drive. With a concentration of home design uses in a walkable environment, and additional investment such as street improvements, signage, and branding of the district, the Design District is intended to be a local and regional destination that attracts shoppers, architects, builders, designers and interior decorators for all of their home design and furnishings needs. The auto service uses in this district are intended to transition to specialty retail/home design uses over the long term.



Around the country, Design Districts are reliable catalysts for redevelopment by building on an existing concentration of home improvement product sales and service and reconverting underutilized buildings into design and home improvement retail spaces, restaurants and cafés. Public and private investment in this district could “kick start” new development in the rest of the Specific Plan area.

C. Use Regulations

1. **Permitted and Conditionally Permitted Uses.** Table 4-1 identifies the permitted and conditionally permitted uses in the Specific Plan Districts. Permitted uses shall be subject to the issuance of a Planned Development Permit or zoning clearance, as described in Chapter 3 and provided for in Chapter 9.25 the Westlake Village Municipal Code. Conditionally permitted uses shall be subject to the approval of a Conditional Use Permit pursuant to the provisions of Chapter 9.26 of the Westlake Village Municipal Code. If a use is not specifically listed in Table 4-1, that use is prohibited. However, the Planning Director or designee shall have the authority to determine whether the proposed use is permitted based on the finding that the use is similar to and no more detrimental than a particular use permitted in the zoning district.
2. **Prohibited Uses.** The following new uses are expressly prohibited in the Specific Plan area:
 - a) Adult entertainment.
 - b) Commercial cannabis land uses and activities.
 - c) Drive-through lanes.
 - d) Free-standing fast food restaurants.
 - e) Service stations.
 - f) Motor vehicle repair and washing facilities.
 - g) Outdoor storage.
3. **Nonconforming Uses.** Any use within the Specific Plan boundary that is nonconforming to the provisions of this Specific Plan shall be subject to Chapter 9.22 (Nonconforming Buildings and Uses) of Article 9 of the Westlake Village Municipal Code, with the following exception:
 - a) Regardless of the provisions conveyed in Chapter 9.22 of Article 9 of the Municipal Code, all motor vehicle repair and washing facilities shall become illegal nonconforming uses beginning fifteen years from the effective date of this Specific Plan.

Table 4-1: Use Regulations for Specific Plan Zoning Districts

Land Use Types	Mixed Use Corsa	Mixed Use Lindero	Design District	Office District	Mixed Use Cedarvalley	Additional Regulations
Residential Uses						
Accessory buildings and structures, to a lawfully established multiple-family dwelling or apartment, including recreation rooms.	P	P	--	--	P	
Apartments	P	P	--	--	--	
Home occupations, accessory to dwelling	P	P	--	--	--	WVMC Chapter 9.28
Live-work units	C	--	--	--	--	Section D.6. of this chapter
Multifamily dwellings	P	P	--	--	--	
Residential care facility which serves six (6) or fewer persons, accessory to dwelling	P	P	--	--	--	
Small family day care homes, accessory to dwelling	P	P	--	--	--	
Student housing, accessory to Oaks Christian School	--	--	--	--	P	
Non-Residential Uses						
Amateur radio antennae	C	C	C	C	C	WVMC Chapter 9.15
Indoor entertainment facilities, such as theatres, sports facilities including gymnasiums and health clubs, nightclubs, billiard parlors and amusement facilities.	C	--	C	--	--	
Clubs, lodges and halls	C	C	C	C	C	
Bars, cocktail lounges and wine tasting rooms, including accessory entertainment.	C	--	C	--	--	
Business and communication services - Includes the provision, primarily to firms rather than to individuals, of services of a clerical, goods brokerage, communication or minor processing nature, including multi-copy and blueprinting services; but excludes the printing of books, other than pamphlets and small reports for other firms, and the storage of goods, other than small samples, for sale.	C	C	C	P	P	--
Churches	C	C	C	C	C	

Table 4-1: Use Regulations for Specific Plan Zoning Districts (continued)

Land Use Types	Mixed Use Corsa	Mixed Use Lindero	Design District	Office District	Mixed Use Cedarvalley	Additional Regulations
Consumer repair services - Includes the repair of personal apparel, household appliances, furniture and similar items, but excludes the repair of motor vehicles.	--	--	P	--	--	
Day care facilities	C	C	C	C	C	
Emergency shelters	C	C	C	C	C	WVMC Section 9.10.050
Farmers' market	C	--	C	--	--	
Hospitals, small animal	--	--	C	--	--	
Hotels and motels	C	C	C	C	C	
Instructional studios/facilities	C	C	P	C	C	
Kennels	--	--	C	--	--	
Laboratories, when not acting as an accessory use to a primary use.	--	--	--	C	--	
Medical services - Includes the provision of therapeutic, preventive or corrective personal treatment services by physicians, dentists and other practitioners, as well as the provisions of medical testing and analysis services, but excludes hospitals.	P	P	P	P	P	
Parking lot or parking structure (stand alone)	C	C	C	C	C	
Personal and convenience services - Includes the provision of barber and beauty care, self-service laundromats, community serving retail laundry or community serving dry cleaning establishments and similar services provided that the building is so constructed and the equipment is so installed and maintained and the activity is so conducted that all noise, vibration, dust, odor, and all other objectionable factors will be confined or reduced to the extent that no annoyance or injury will result to persons or property in the immediate vicinity.	P	P	P	P	P	

Table 4-1: Use Regulations for Specific Plan Zoning Districts (continued)

Land Use Types	Mixed Use Corsa	Mixed Use Lindero	Design District	Office District	Mixed Use Cedarvalley	Additional Regulations
Pet grooming and veterinarian services.	--	--	C	--	--	
Professional, medical, financial, administrative, public service and general business offices and accessory uses.	P	P	C	P	P	
Public service and utility structures and facilities.	C	C	C	C	C	
Restaurants (sit down and take-out, no drive-thru lanes)	P	P	P	P	P	
Retail sales:						
a) Food sales - Includes the retail sale of food and beverages for home and/or on-premises consumption.	C	--	P	--	--	
b) General retail services - Includes the retail sale or rental of goods primarily for personal or household use, but excludes the sale or rental of motor vehicles.	--	--	P	--	--	
c) Home improvement sales and service - Includes hardware, building materials, paint, fixtures, design and home furnishings)	--	--	P	--	--	
d) Retail sales and commercial services in conjunction with and incidental to an office or business park use	P	P	P	P	P	
Sale of alcoholic beverages for on- or off-site consumption.	C	C	C	C	--	
Satellite earth station antennae exceeding two (2) meters in diameter and wireless telecommunications antenna facilities.	C	C	C	C	C	WVMC Chapter 9.15
Schools	C	C	C	C	C	
Temporary uses, subject to the issuance of a temporary use permit	P	P	P	P	P	WVMC Chapter 9.29

D. Development Standards

1. **Development Standards.** Tables 4-2 and 4-3 set forth the development standards for the Specific Plan zoning districts. Additional development standards contained in Chapter 9.14 of the Westlake Village Municipal Code also apply and are identified where applicable in Table 4-2. Table 4-2 also identifies the general building entitlements for each zoning district as set forth in Chapter 3 of this Specific Plan. Development of a site and structures shall also be consistent with the design guidelines contained in Chapter 5 of this Specific Plan.
2. **Residential Density in Mixed Use Development Projects.** The residential portion of mixed-use projects must be attached units with a maximum density of 30 dwellings per net acre in the Mixed Use Corsa Zoning District and 35 dwellings per net acre in the Mixed Use Lindero Zoning District subject to increase due to density bonus provision pursuant to State law. Residential density in a mixed-use project shall be calculated as the number of dwellings divided by the portion of the project area in acres devoted to such use, including when such use is part of a vertical mixed use development.
3. **Nonconforming Buildings.** Any building within the Specific Plan boundary that is nonconforming to the provisions of this Specific Plan shall be subject to Chapter 9.22 (Nonconforming Buildings and Uses) of Article 9 of the Westlake Village Municipal Code, with the following exception:
 - a) Four parcels on Cedarvalley Drive exceed the maximum floor area ratio permitted by this Specific Plan. These parcels are identified with the following Assessor's Parcel Numbers (APNs): 2054031082, 2054031029, 2054031020, and 2054031030. These buildings shall be allowed to remain indefinitely at their current square footage and make improvements, with the exception of expanding the building square footage.

Table 4-2: Development Standards for Specific Plan Zoning Districts

Development Standards	Mixed Use Corsa	Mixed Use Lindero	Design District	Office District	Mixed Use Cedarvalley	Additional Regulations
Maximum Floor Area Ratio (FAR) ¹	0.5	0.5	0.5	0.5	0.5	
Maximum Residential Density	30 du/acre	35 du/acre	--	--	--	
Maximum Building Coverage	40%	40%	40%	40%	40%	
Maximum Building Height	55 feet, 3 stories	70 feet, 5 stories	35 feet, 2 stories	70 feet, 5 stories	35 feet, 2 stories	
Minimum setback (landscaped) from an abutting public right-of-way	--	--	--	--	--	See Table 4-3 in this chapter
Minimum interior yard setback (landscaped) ²	10 feet	10 feet	10 feet	10 feet	10 feet	
Minimum Residential Common Open Space Area ³	100 sf/du	100 sf/du	--	--	--	
Minimum Residential Private Open Space Area	50 sf/du	50 sf/du	--	--	--	
Minimum Non-Residential Common Open Space ⁴	1,000 sf/acre	1,000 sf/acre	--	1,000 sf/acre	--	
Additional Usable Public Open Space Along Ridgelines ⁵	20-foot average depth; 8-foot minimum	--	--	--	--	
Projections Permitted into Required Yards	--	--	--	--	--	WVMC Chapter 9.14
Accessory Structures and Equipment	--	--	--	--	--	WVMC Chapter 9.14
Fences, Walls and Screening	--	--	--	--	--	WVMC Chapter 9.14
General Building Entitlements						
Allowable Building Square Footage ⁶	86,780	115,790	538,255	230,000	288,961	
Allowable Dwelling Units ⁶	301	716	--	--	--	

Table 4-2 Notes:

¹The square footage devoted to parking structures shall not apply to calculations of floor area ratio.

²If a unified development project covers more than one parcel, the interior yard setback requirements apply to the entire development project, not the individual parcel.

³Minimum dimension shall be 20 feet for common open space. More than one common open space area may be provided. The sum of square footages for all eligible open space areas shall comprise the total open space requirement. Required interior yard areas may be included in the calculated open space area. Common open space areas shall have no parking, driveway or right-of-way encroachments.

⁴Required in the Mixed Use Corsa, Mixed Use Lindero and Office zoning districts for new projects with a lot area of one acre or more.

Table 4-2 Notes (continued):

⁵This standard applies to four parcels in the Mixed Use Corsa zoning district (APN 2054029042, 2054029043, 2054029044, 2054029045) and one parcel in the Business Park West zoning district (APN 2054031081), which are located along the ridgeline and have southerly views. For these parcels, public open space is required along the ridgelines in the form of a connected linear park with an average depth of 20 feet and minimum depth of 8 feet, but increased depths may be required depending on layout of buildings and other physical improvements. Each linear park shall be designed to take advantage of the views and provide passive open space for employees, customers, and residents. Features of the linear parks should include walkways, seating, small terraces, and landscaping to beautify the area.

⁶Minor reallocation of allowable building square footage and number of dwelling units may be permitted between zoning districts provided that total allowable building square footage and number of dwelling units is not increased. The building entitlements for the Business Park West and East zoning districts is set forth in Table 3-1 in Chapter 3. The allocation of building square footage for the Design District is 363,440 square feet in the Design District North and 174,815 square feet in the Design District South. The allocation of building square footage for the Business Park zoning district is 242,047 square feet for Business Park West and 129,559 square feet for Business Park East. The Business Park zoning district is regulated by Chapter 9.10 of the Westlake Village Municipal Code.

Table 4-3: Minimum Setback from Specific Plan Streets

Street	Minimum Building Setback
Thousand Oaks Boulevard	30 feet
Lindero Canyon Road	30 feet
Via Colinas	20 feet
Via Rocas	20 feet
La Tienda Road	100 feet
La Baya Drive	10 feet
Corsa Avenue	10 feet
Cedarvalley Drive	10 feet

- 4. Off-Street Parking Requirements.** Table 4-4 identifies the parking requirements for allowable land uses in the Specific Plan area. Required parking for all other uses not listed herein shall conform to criteria set forth in Article 9 (Zoning

Regulations) of the Westlake Village Municipal Code, Chapter 9.19 (Off-Street Parking and Loading Standards). The required parking shall be provided on the parcel containing the use, or on a different legal parcel, provided that all of the spaces are within acceptable walking distance of the building entrance of any use, and shared parking covenants and easements are in place.

Parking requirements may be reduced after a detailed parking demand analysis and parking management plan have been designed and approved by the Planning Director and City Engineer. For mixed-use or non-residential development, a parking study shall be required from an applicant aiming to reduce their minimum project parking requirement. The study shall propose the use of parking alternatives for non-residential uses, such as valet, on-street parking, and/or shared parking. For instance, residential guest parking might be shared with non-residential uses during the daytime. In addition, close proximity to a transit stop (1/4 mile) could also be used as means to justify the reduction in minimum parking requirements for non-residential uses. It is recommended that a reduction of no more than 25% of the total parking spaces required for non-residential uses be granted. If changes to the uses occur at some future time, an updated parking study will be required to reflect the new uses.

5. Bicycle Parking Requirements. Parking spaces for bicycles shall be provided as follows:

Table 4-4: Off-Street Parking Requirements

Land Use	Number of Parking Spaces
Business Park	One (1) space per 330 square feet of gross floor area. Business park includes uses such as light industrial, wholesaling, home furnishings, research and development, etc.
General Office	One (1) space per 290 square feet of gross floor area.
Retail and Services	One (1) space per 250 square feet of gross leasable area.
Restaurant	One (1) space for each 250 square feet of gross floor area. Restaurant uses includes quality restaurants, sit-down restaurants.
Residential	1.85 spaces (minimum 1 covered space) per dwelling unit including resident and guest parking. Residential uses include lofts, apartments, townhouses.

- a) For any building, portion thereof or addition to, used for non-residential purposes which contains a floor area in excess of 10,000 square feet, bicycle parking spaces shall be provided at the rate of two percent of the number of automobile parking spaces required. If the calculation of the number of required spaces results in a number including a fraction, the next highest whole number shall be the number of spaces required.
- b) For multi-family residential developments without enclosed garages, a

minimum of one hundred (100) cubic feet of lockable storage areas shall be provided outside each dwelling unit, with a minimum dimension of thirty (30) inches. The design, location and size of the storage space shall be subject to the approval of the Planning Director.

- c) For non-residential development, one long-term bicycle storage space per 5,000 square feet shall be required for buildings of 25,000 square feet or more. The storage space can either be provided in bicycle lockers or within a secured room within said building.
- d) All bicycle parking spaces required shall include a stationary parking device which adequately supports the bicycle. In addition, at least half of the bicycle parking spaces shall include a stationary parking device which securely locks the bicycle without the use of a user-supplied cable or chain. Devices which hold the bicycle upright by wheel contact must hold at least 180 degrees of wheel arc.
- e) Each bicycle parking space shall be a minimum of two feet in width and six feet in length and shall have a minimum of six feet of overhead clearance.
- f) Bicycle parking spaces shall be located no farther than the distance from a main entrance of the building to the nearest off-street automobile parking space.
- g) Bicycle parking spaces shall be separated from automobile parking spaces or aisles by a wall, fence, or curb or by at least five feet of open space marked to prohibit parking.
- h) Aisles providing access to bicycle parking spaces shall be at least five feet in width.
- i) Signage which is clearly legible upon approach to every automobile entrance to the parking facility shall be displayed indicating the availability and location of bicycle parking.
- j) To encourage bicycle commuters, a minimum of two single occupancy showers and locker facilities shall be provided and accessible to all employees in new buildings and in existing buildings with at least 50,000 square of floor area for office, commercial, business, professional and industrial uses, and at least 100,000 square feet of floor area for retail uses.

- 6. Standards for Live-Work Units.** A live-work unit is defined as an integrated living unit and working space with an internal connection between the living and working space, occupied and utilized by a single housekeeping unit. Live-work units may be designed for existing commercial or residential structures as adaptive reuse, or for new development specifically designed and constructed for such use. Live-work units may also occupy a portion of a mixed use development. Live-work units shall be developed according to the following regulations:

- a) The minimum floor area of a live-work unit shall be 750 square feet.
- b) All living space within the live-work unit shall be contiguous with, and an integral part of, the working space, with direct access between the two areas.
- c) At least one of the full-time workers of the live-work unit shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live-work unit may have employees in addition to residents as necessary.
- d) Access to individual units shall be from common access areas, corridors or hallways.
- e) Complete kitchen space and sanitary facilities shall be provided in compliance with all applicable codes.
- f) The workspace shall not occupy more than 40% of the unit.
- g) Ancillary retail space may be integrated with working space.

E. Performance Standards

The purpose of this section is to ensure that residential uses in mixed-use zoning districts are not adversely impacted by adjacent commercial uses, including but not limited to traffic, noise, light, and safety impacts.

1. **Hours of Operation.** The hours of operation for commercial uses shall be no earlier than 7:00 a.m. and no later than 10:00 p.m. daily, unless modified by a Conditional Use Permit.
2. **Loading.** Loading, unloading and all maintenance activities shall be conducted within the hours of operation noted above so as not to create a nuisance to adjacent residents and tenants, and in conformance with Chapter 4.4 (Noise) of the Municipal Code.
3. **Noise.** Noise generated by activities on the premises shall be controlled in such a manner consistent with Chapter 4.4 (Noise) of the Municipal Code, and so as not to create a nuisance on any adjacent property. Residential units shall be constructed and designed to reduce noise. Proper design may include, but shall not be limited to, building orientation, double windows, wall and ceiling insulation, and orientation of vents. Common walls and floors/ceilings between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.
4. **Light and Glare.** All outdoor lighting associated with commercial uses adjacent to or within the immediate vicinity of residential uses shall be designed to illuminate commercial uses, while minimizing light trespass into residential areas. Lighting for

nonresidential uses shall be appropriately designed, located, and shielded to ensure that they do not negatively impact the residential uses in the development or any adjacent residential uses.

5. **Limitations on Activities.** Any activity or use as determined by the Planning Director not to be compatible with residential activities and/or to have the possibility of affecting the health or safety of residents due to the potential for the use to create dust, glare, heat, noise, noxious gases, odor, smoke, traffic, vibration or other impacts, or create a hazard because of materials, processes, products, or wastes shall not be permitted within any mixed use zoning district.
6. **Security.** The residential units shall be designed to ensure the security of residents through the provision of separate and secured entrances and exits that are directly accessible to secured parking areas. Where residential units are in the same structure as a commercial use, access to residential units shall be from a secured area located on the first floor at the ground level. Nonresidential and residential uses located on the same floor shall not have common entrance hallways or common balconies.

F. Other Applicable Zoning Regulations

In addition to the development requirements contained in this Specific Plan as noted above, regulations contained in Article 9 of the Westlake Village Municipal Code (Planning and Zoning Regulations) may also apply. It is the responsibility of the property owner to comply with all standards of this Specific Plan and Article 9 as applicable, including, but not limited to:

- Design Standards (Chapter 9.15)
- Landscaping Standards (Chapter 9.16)
- Signs (Chapter 9.18)
- Oak Tree and Heritage Tree Preservation Standards (Chapter 9.21)
- Planned Development Permits (Chapter 9.25)
- Conditional Use Permits (Chapter 9.26)
- Variances (Chapter 9.27)
- Home Occupations (Chapter 9.28)
- Art in Public Places (Chapter 9.39)

A. Introduction

This chapter provides design guidelines for commercial, mixed use and attached residential development in the North Business Park Specific Plan area. While the North Business Park is intended to have a more urban land use intensity than the low-intensity character of the majority of the City, building design should be compatible with the City's established suburban character and environmental setting. The guidelines address design considerations for achieving pedestrian and human-scaled development in a "village" character consistent with the objectives of the General Plan. In addition, sustainability measures that help reduce environmental impacts, promote energy efficiency, and facilitate a healthier environment are incorporated throughout in the design guidelines. These design guidelines are intended to:

- Maintain the City's tradition of high quality architecture and landscape design.
- Promote compatibility in scale, mass, form, character, and quality with the existing character of the City.
- Promote compatibility with the City's natural environmental resources, viewsheds, and open spaces.
- Create visual interest in individual buildings, while maintaining a sense of harmony within the project.
- Promote design creativity and variation while ensuring consistency in building scale, proportion and overall character.
- Facilitate pedestrian movement and connections within and between mixed use, commercial and residential developments.
- Encourage environmental sensitivity in development.

B. Design Review

All new development and improvements greater than 50% of the existing building square footage will be subject to professional Design Review by the City, in conjunction

with a Planned Development Permit, and all new development projects and substantial landscape improvements will be subject to professional Landscape Design Review. Design evaluation shall be based on substantial compliance with the intent and guidelines set forth in this chapter. While the guidelines are not absolute requirements, applicants should adhere to each of the guidelines unless it is determined by the review body that a guideline is not feasible and/or appropriate for a specific project. The design guidelines in this chapter are in addition to the design standards contained in Chapter 9.15 (Design Standards) of the Westlake Village Municipal Code. The City's landscape design standards set forth in Chapter 9.16 (Landscaping Standards) of the Municipal Code shall be used for landscape design review.

C. Building Siting and Orientation

1. Provide integrated site planning by designing for the functional and visual integration of building(s), services, vehicular access and parking, and outdoor public spaces.
2. Locate and design buildings to facilitate and encourage pedestrian activity and convey a visual link to the street, sidewalks and open space areas.
3. Locate and orient building entrances and windows to create a strong and direct relationship with the street, provide visual interest, and encourage pedestrian activity. Avoid developments that are internally focused at the expense of an active street environment.
4. Locate parking to be less obtrusive and not fronting streets, where feasible. Site buildings so that they screen parking and unsightly activities from public view.
5. In larger projects, cluster buildings to create usable pedestrian areas and open space. When possible, design buildings with shared open spaces, plazas, and courtyards that can serve as outdoor eating and gathering places.
6. In residential projects, cluster units or break up into groups of structures, and avoid the use of long access balconies and corridors. Where clustering is impractical, provide a visual link between separate structures through the use of an arcade system, trellis or other open structure.
7. Design vehicular access to ensure minimum conflict between pedestrians, automobiles, and service vehicles. Sight lines, pedestrian walkways, and lighting are factors to consider in final site design.
8. Orient individual buildings in multi-family and mixed use projects to promote privacy to the greatest extent possible. Offset or stagger windows, balconies or similar openings above the first story so as not to have a direct line-of-sight into adjacent units within the development. In mixed use projects, locate residential windows so that they face away from loading areas and docks.

9. In mixed use districts and projects, protect residential uses with a sensitive transition in building location and massing. For example, locate the building massing of non-residential buildings away from residential uses; maintain lower profiles at residential edges.
10. Whenever adjacent residential and commercial uses can mutually benefit from connection rather than separation, provide appropriate connective elements such as walkways, common landscaped areas, building orientation, gates and/or unfenced property lines.
11. Site and orient buildings to maximize the use of sunlight and shade for energy savings, and to avoid blocking solar access to adjacent properties.
12. Protect view corridors and frame views of the ridgelines, mountains and hillside areas, particularly from outdoor plazas and open space areas.
13. Integrate new development with the natural topography and avoid altering landforms and removing trees.



Three-dimensional concept models illustrate the important function building orientation and site layout play in creating useable open spaces, pedestrian gathering areas and plazas in larger development projects. In the Mixed Use Corsa, Mixed Use Lindero and Office zoning district, where larger redevelopment projects are most likely to occur, new development will include large plazas, shared open space areas, employee gathering spaces, and a large “village green” for resting, gathering, events and activities

D. Building Form and Façades

1. Design new development so that the scale and mass is similar to that of neighboring developments and does not overwhelm them with disproportionate size or incompatible design.
2. Wall and roof planes should be varied and articulated into smaller modules that reduce the overall massing and scale of building and add richness and variety. Attenuate the appearance of building mass with arcades, courtyards, pergolas, color and material changes, and stepping stories back above the ground level.
3. Variations in roof type, style, and height should be used to provide visual interest and to reduce the overall mass of the building. Overly complex and distracting roofs are not encouraged.
4. Use building form and articulation to emphasize and clearly demarcate the main entrance. The main building entrance should be easily distinguished from the rest of the building.
5. Use building form and articulation to emphasize individual units within a residential building, and to emphasize foyers, lobbies, and reception areas within commercial projects. Use the following design elements to articulate buildings and give individual identity to each vertical module or individual unit within a building:
 - Provide a deep notch (minimum one-foot) between the modules,
 - Vary architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type),
 - Provide porches and balconies, and
 - Vary color or materials of each individual module within a harmonious palette of colors and materials, etc.
6. Employ vertical architectural elements and strong building forms such as towers, angled corners, or other architectural features at corner locations, to provide focal points, and demarcate important gateways.

Well-articulated buildings create a more welcoming environment than uniform, nondescript structures. Buildings with angled corner elements, towers or other architectural elements create a focal point.



7. Incorporate architectural details and façade elements that will create a human scale at the street level, such as awnings, canopies, arbors, and trellises, which also provide shade over public sidewalks and walkways. The appropriate use of other architectural details, including reveals, course lines, decorative cornices, columns, etc., is also encouraged as a means of creating interest, variety, and distinctive design.
8. Stairs, balconies, porches and patios should be designed such that they are integrated into and complement the overall design of the building.
9. The form and scale of multi-family development should emulate the best characteristics of the existing residential neighborhoods in the City. Avoid massive apartment-type structures in larger multi-family developments, including plain box shapes, large monotonous façades, and long straight line building fronts. Massing should articulate individual units or clusters of units through varied heights and setbacks.
10. Step back the upper stories of new multi-family buildings to scale down façades that face the street, common space, and adjacent residential structures.
11. Windows and doors should be proportionate in scale to the building elevation. Windows should be articulated with accent trim, sills, kickers, shutters, flower boxes, awnings, or trellises authentic to the architectural style of the building. Where appropriate to the architectural character of the building, windows should be inset from building walls to create shade and shadow detail appropriate to the architectural style.
12. Use transparent glass and avoid the use of dark-tinted, reflective and opaque glass. Avoid excessive displays or signs on the windows to leave the interior of the stores visible from the street.



Retail storefronts in the Design District should have large display windows oriented toward the street to create a vibrant street and promote a pedestrian environment.

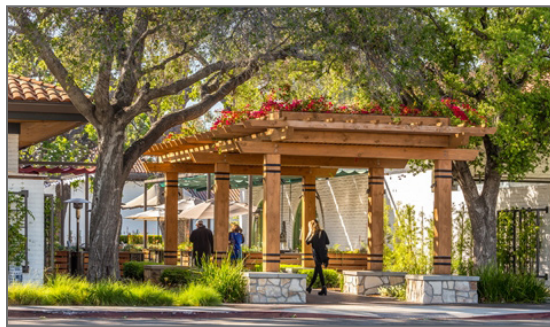


Residential and office buildings should also provide a maximum degree of window openings to provide visual interest and eyes on the street.

E. Pedestrian Connectivity

1. Design new development to enhance pedestrian movement within the project and to adjacent uses to promote a more walkable environment.
2. Provide pedestrian walkways to connect buildings on a site to each other, to on-site automobile and bicycle parking areas, and to any on-site open space areas and amenities. In multi-family residential projects, design pedestrian walkways to link dwelling units with common open space and recreation areas, parking areas, and the street at the project entries.
3. Where feasible, use arcades, covered walkways, and trellises to provide shaded and sheltered pedestrian circulation.
4. Pave pedestrian paths or walkways to building entrances with high quality paving materials such as but not limited to, stone pavers, decorative concrete pavers and brick. The use of asphalt for paving walkways is prohibited.
5. Where pedestrian paths or walkways cross parking areas or driveways, provide decorative paving or some other method to define the pedestrian space and delineate crossings.
6. Incorporate lighting along sidewalks or other pedestrian walkways to enhance the pedestrian environment and provide for a safe environment. Lighting shall be low mounted and downward casting in a manner that reduces light trespass onto adjacent properties.
7. All buildings should have a clear path of travel to the main entry which is universally accessible (no vertical or horizontal barriers to wheelchairs). Universally accessible entries shall be provided for all buildings, including residential structures.
8. Consider curvilinear and off-set paths, which provide a more inviting and interesting experience and reflect the non-linear nature of the streets in the area.

Pedestrian walkways that are well-defined by landscaping, lighting, and decorative paving should be used throughout new development in the North Business Park. Walkways, paths, and pergolas can provide shade, connections, and define usable space.



F. Plazas and Courtyards

1. Incorporate well-defined outdoor spaces, such as plazas, courtyards, patios, and terraces, in the design of commercial and mixed-use developments. All outdoor spaces and associated amenities shall be continually maintained by the property owner.
2. Define outdoor spaces with an articulated edge through a combination of building design, landscaping and accent trees, and specialty paving. Discourage oversized spaces that lack containment.
3. Provide direct access to public plazas and courtyards from the street; secondary access may be provided from offices, stores, restaurants, residences or other uses within the development.
4. Link adjoining outdoor spaces with comfortable paths and walks to create a network of spaces.
5. Include landscaping, seating, and other site amenities in plaza and courtyard design; create focal points with the use of public art or a water feature using recycled water.
6. Use planters and pots to complement the project architecture and other site amenities; avoid obstructing pedestrian traffic flow with planters and pots.
7. Provide shade trees or architectural elements that provide shelter and relief from direct sunlight.
8. Design outdoor space with safety in mind by promoting visibility from the street, parking areas, and building windows, and providing architecturally compatible lighting to enhance nighttime security.
9. Buffer plazas and courtyards from parking areas and drive aisles.
10. Design plazas, courtyards and walkways to collect stormwater where feasible.



Plazas, courtyards, and pedestrian areas function as “oases” or outdoor rooms in mixed use and commercial developments. Decorative site amenities, such as seating, planters and pots, fountains, or water features are critical design components for creating a pleasant pedestrian-oriented environment.

G. Outdoor Dining

1. Locate patio dining within private property adjacent to the streets or public walkways. Outdoor patio dining is allowed within the right-of-way setback where right-of-way setbacks occur.
2. Locate sidewalk dining within the public sidewalk rights-of-way, subject to the clear widths available, immediately adjacent to the buildings with the pedestrian path immediately along the curb. Approval of Encroachment Permit is required from the City Engineer for sidewalk dining.
3. For sidewalk dining, maintain a clear pathway for pedestrians, free from all obstructions, of not less than 4 feet depending on the exact sidewalk width. The City Engineer may grant exemption to the minimum requirements based on the particular site conditions.
4. For sidewalk dining, separate the dining area from the remainder of the sidewalk by a temporary physical barrier, such as bollards, planter boxes or pots, umbrellas, movable fencing/railing, etc. Use materials compatible in style, color and finish with the adjacent structure. Remove all outdoor barriers from the sidewalk at the close of the business establishment daily.
5. Demarcate patio dining spaces adjacent to streets or walkways by either temporary or permanent boundary definers such as railings, fencing, landscaping in planter boxes, low wall, or bollards. Use materials compatible in style, color and finish with the adjacent structure.
6. The area used for outdoor dining shall not be included in the restaurant's total square footage for the purposes of calculating the parking requirement.
7. Follow all federal and state requirements as per the Americans with Disability Act and California Title 24 Accessibility Guidelines.

Outdoor dining adds interest and vibrancy to commercial and mixed use areas and will contribute to increased pedestrian activity in the North Business Park.



H. Open Space in Multi-Family Developments

1. Design and orient common and private open space areas to take advantage of available sunlight and shelter from the noise and traffic of adjacent streets or other incompatible uses.
2. Locate common open space areas convenient to the majority of dwellings and secure and visible from dwellings to ensure safe use.
3. In common open space areas, provide amenities appropriate to the project's size; for example, pools, spas, play areas, and recreation buildings are encouraged for larger projects, while barbeque areas and gazebos may be more appropriate for smaller projects.
4. Locate private useable open space contiguous to the residential unit served and screen from public view for privacy. Substantially enclose patios that front a public street and provide solid balcony rail on balconies that front a public street for screening and privacy.
5. Accessible rooftop open space may be used as common useable open space by the residents. It may also be used for private useable open space, when the space is directly accessible from the unit it serves.



Common open space areas should be integral to the design of multi-family developments. Building layout, landscaping, pedestrian walkways, and active recreational amenities contribute to the quality of the common open space area for residents.

I. Architectural Style

1. Provide appropriately scaled and proportioned façade elements consistent with the architectural style. The elements of a building façade should relate logically to each other to enhance the architectural characteristics of the building.
2. Design visually attractive buildings that add richness and variety to the community, while providing compatibility with surrounding development.
3. Architectural styles should be accurate and appropriate for the building typology. Contemporary adaptation of traditional vernacular is acceptable. Although historical architectural vernacular is encouraged, direct replication or mimicry is discouraged.
4. Incorporate appropriately scaled windows, doors, and other building details consistent with the style of architecture to achieve stylistic coherence.
5. Avoid excessive and overly gratuitous ornamentation that detracts from the visual clarity of the architecture.
6. Buildings should incorporate 360-degree architecture, to create a building that is aesthetically pleasing from all angles. Employ high quality architectural treatment of all prominent building elevations.
7. Provide appropriately scaled and proportioned façade elements consistent with the architectural style. The elements of a building façade should relate logically to each other to enhance the architectural characteristics of the building.

Recognizable and consistent architectural styles create a sense of place and add visual interest to an area. The goal is to complement the architectural styles commonly found in Westlake Village while still encouraging innovation and creativity in design. Architectural styles vary in the community; common styles include Spanish Revival, contemporary, and “Napa Valley-style” as seen in the new Shoppes at Westlake Village.



J. Materials, Finishes and Colors

1. Use high quality materials and finishes suitable to the architectural style, scale, character and design theme of the building.
2. Treat buildings as a whole and finish appropriately on all sides to provide continuity; avoid piecemeal embellishment and frequent changes in materials.
3. Design architectural features to be an integral part of the building and avoid ornamentation and features that appear “tacked on” or artificially thin; materials tend to appear substantial and integral to the structure when material changes occur at changes in plane.
4. Use natural materials whenever possible; avoid synthetic materials and veneers to simulate wood, masonry, stone and brick. Natural roofing materials, such as clay tiles and slate, should be left in their natural color. Avoid the use of materials with glossy surfaces that appear unnatural.
5. Use sustainable, high quality building materials that have a long-life span and are not energy-intensive to manufacture, and when feasible, use building products made from recycled materials.
6. Limit the number of materials and colors used on the exterior of an individual building so that there is visual simplicity and harmony. Colors consistent with the prominent “earthen” colors of the hills surrounding the Westlake Village community and colors traditional to architectural themes previously approved in Westlake Village are required.
7. Relate the transition between base and accent colors to changes in buildings materials or the change of building surface planes; colors should not meet or change without some physical change or definition to the surface plane.
8. Consider the provision of a green roof to reduce solar gain (which contributes to the urban heat island effect) and to reduce the quantity of water entering the storm drain system. Consider solar panels or tile roof solar panels to capture solar energy for internal use of the project.
9. See also Section 9.15.030.A. (Exterior Materials and Colors) of the Westlake Village Municipal Code.



Natural, high quality materials and earthen colors complement the natural setting and character of the community.

K. Exterior Lighting

1. Provide exterior lighting for the security and safety of on-site areas such as building entrances, parking, loading, pedestrian walkways, address numbers, and open space areas. Decorative night lighting is required.
2. Design light fixtures and their structural support to be architecturally compatible with the main buildings on-site.
3. Avoid direct glare onto adjoining property, streets, or skyward. Shield all lighting fixtures to confine light spread on-site, and direct to illuminate only the areas and elements intended, such as paths, entryways and focal elements.
4. In mixed use zoning districts, appropriately shield all exterior lighting so as not to spill over into or otherwise adversely impact residential uses.
5. Use low energy lights, such as LED lights or solar powered lights, whenever possible.
6. The selected light fixtures shall adhere to the guidelines set forth by the Dark Sky Association to protect the area's view of stars and the needs of nocturnal wildlife.
7. See also Section 9.15.030.B. (Exterior Decorative Illumination) of the Zoning Code.
8. For parking lot lighting, see Section 9.19.080.H. (Lighting) of the Zoning Code for minimum foot candle requirements for parking areas.

Exterior lighting should accent the building, compliment architectural details, and enhance the enjoyment of the nighttime environment.



L. Service Areas and Mechanical Equipment

1. Consider services facilities, loading areas, mechanical equipment, and refuse and recyclable collection facilities as an integral part of the site plan.
2. Locate service and loading areas away from public streets and spaces and design to avoid conflicts with pedestrians and other vehicles.
3. Screen mechanical, ventilating, and security equipment from public view and locate it away from the street edge. This includes all ground, wall and roof mounted equipment. Roof mounted equipment may require extra screening to adequately conceal the view of roof mounted equipment from the Westlake Village Community Park.
4. Design screening elements to be an integral part of the architecture of the building and avoid giving the appearance of being “tacked on.”
5. Provide equal capacity of enclosed and separate refuse and recyclable collection facilities for the residential units and commercial uses in a mixed use development; locate all refuse collection facilities for commercial uses where associated odors and noise will not adversely impact the residential uses.
6. Enclose refuse and recyclable collection areas on three sides by a 6-foot tall, decorative, capped, masonry wall. The wall materials shall be complementary to the architectural components of the development they serve. Enclose the fourth side of the collection area with an opaque, self-latching gate.
7. Cover all trash collection areas to prevent rain from falling on containers or the enclosure floor and carrying contaminants to the stormwater system. The cover/ roof may be part of the solid waste enclosure or the roof of a building. The roof canopy should extend sufficiently outward in all directions so that wind-blown rain will not enter the interior of the storage area.
8. Outdoor storage of materials is prohibited.
9. See also Sections 9.14.040. (Accessory Structures and Equipment) and 9.14.050. (Fences, Walls and Screening) of the Westlake Village Municipal Code.



Mechanical equipment, refuse facilities and storage areas should be screened from public view with architecturally compatible walls, fences, and landscaping.

M. Parking Lots

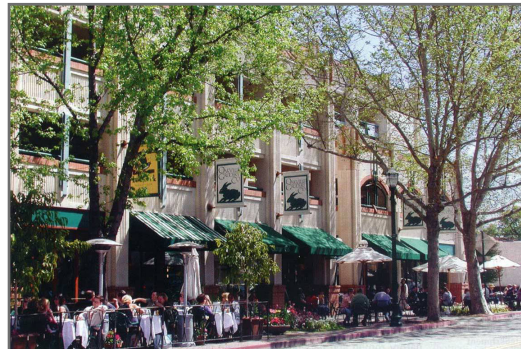
1. Locate surface parking on the interior side or rear of the site to the greatest extent practicable.
2. Screen views of automobiles from public view and adjacent sensitive land uses with a minimum 3-foot high wall or landscape screen; the visual impact of parking lots can also be mitigated and screened by buildings and site landscaping.
3. Use landscaping to minimize the expansiveness of surface parking lots, shade surface parking, and reduce heat build-up.
4. Landscape parking lots with an evenly distributed mix of canopy shade trees with shrubs and/or ground cover plants. Landscaping of parking areas shall be provided and maintained according to the regulations contained in Chapter 9.19 of the Westlake Village Municipal Code
5. Consider the use of permeable paving and bioswales in parking lot design.
6. Separate parking areas from buildings by a pedestrian walkway and/or landscape strip.
7. Design the main entry driveway to be easily identifiable, incorporating landscaping and accent paving that is related to the building hierarchy and color.
8. Building siting and parking design should maximize opportunities for shared parking, access entries and driveways between adjacent sites.

Parking lot landscaping helps screen parking and provides a comfortable shaded environment. The use of bioswales and permeable paving in parking lot design provides the added benefit of retaining storm water runoff on-site.



N. Parking Structures

1. Design the massing, height and architectural design of the parking structure to be consistent and compatible with the rest of the development within which the parking structure is to be located.
2. Design vehicular access into parking structures so as not to obstruct free flow of traffic on adjoining public streets.
3. Minimize the visual identity of a parking structure by providing one or more of the following in design of the structure:
 - Architectural detailing and articulation that provides texture on the façade and effectively integrates the parking structure into the basic building design;
 - Landscape material (e.g., foundation plantings, vertical trellis with vines, planter boxes with cascading landscape material) that results in the parking structure being adequately screened from adjoining parcels and public rights-of-way;
 - Artwork such as sculptures, murals, and mosaics.
4. Parking structure design should include flat decks rather than sloping decks to facilitate re-use as habitable buildings should a future reduction in parking demand occur based on increased use of autonomous vehicles or other emerging technologies. Maintain horizontal floor lines throughout the exterior façade.
5. Provide high light levels in parking structures, particularly in pedestrian areas such as stairs, elevator lobbies, and entrances. Light colored ceilings and walls are also recommended to increase light.
6. Eliminate glare and visibility of pole mounted light fixtures on upper decks by using full cutoff fixtures and minimizing pole heights to 16 feet. Locate between internal parking rows rather than at the structure's perimeter.



Parking structures should be designed to be integrated with the new development. Well-designed structures, ground-level storefronts, and enhanced landscape treatments can minimize the visual impact of parking structures.

A. Introduction

The recommended circulation and parking improvements for the North Business Park Specific Plan stem from and build upon the roadway network systems that are currently in place, as well as provide for circulation, parking and connectivity improvements to be correlated with build-out of the Specific Plan. Continued integration with the street network and suburban nature of the roadway system are important objectives in planning for future circulation by all travel modes. Development of a comfortable pedestrian environment through establishment of better pedestrian connections is also an essential objective. Improvements and changes have been identified in order to improve overall circulation throughout the Specific Plan area, accommodate new activity, and to plan for adequate parking.

B. Context

The existing Specific Plan roadway network is an irregular, non-linear (i.e., non-grid) system of streets which provide access on the periphery of the Specific Plan and within the individual subareas of the Specific Plan. Principal traffic-carrying roadways in the area include Thousand Oaks Boulevard which borders the Specific Plan to the north and Lindero Canyon Road which borders the Specific Plan to the east. These two roadways are identified as principal traffic-carrying due to their carrying capacities in serving local and regional traffic, as well as the inter-connection with US Highway 101 for Lindero Canyon Road. In addition, Thousand Oaks Boulevard provides principal connections to communities located east and west of the Specific Plan area, and Lindero Canyon Road provides principal connections to portions of Westlake Village located south of US Highway 101 and areas to the north of the Specific Plan area.

The existing roadways located within the Specific Plan boundary directly serve the interior parcels of the business park. Most of the interior roadways such as Via Rocas,

La Baya Drive, Corsa Avenue, La Tienda Drive and Cedarvalley Drive are discontinuous and local traffic-serving streets only. The discontinuous nature of the interior Specific Plan roadways and the topography of the northerly and central portions of the Specific Plan area generally limit roadway network connections and changes. In addition, it is noted that Via Colinas accommodates both local and regional traffic through the Specific Plan area, and in particular provides a direct connection between Thousand Oaks Boulevard and Lindero Canyon Road as well as US Highway 101

The existing Specific Plan roadway network generally functions well in terms of facilitating vehicular traffic. The City of Westlake Village has been proactive in implementing roadway capacity enhancements and traffic signal system improvements so as to provide and maintain good levels of service and to address traffic circulation issues with respect to traffic volumes and/or congestion. However, pedestrian amenities such as sidewalks and appropriate lighting, bicycle routes or lanes, and bus transit stops are not provided within the Specific Plan on the interior roadways. Accordingly, it is recommended that the Complete Streets concept be employed when considering improvements to the local street system. Complete Streets requires a balanced, multimodal transportation network that meets the needs of all users of streets.

C. Complete Streets

Complete Streets is a national movement to ensure transportation planners and engineers consistently design and operate the entire right-of-way with all users in mind—including motorists, bicyclists, public transportation users, and pedestrians of all ages and abilities. In the State of California, it's not just a movement, but a requirement by law.

Former Governor Arnold Schwarzenegger signed the “*California Complete Streets Act of 2008*” (AB 1358) into law on September 30, 2008. Commencing January 1, 2011, the bill requires, “*that the legislative body of a city or county, upon any substantive revision of the circulation element of the general plan, modify the circulation element to plan for a balanced, multimodal transportation network that meets the needs of all users of streets, roads, and highways, defined to include motorists, pedestrians, bicyclists, children, persons with disabilities, seniors, movers of commercial goods, and users of public transportation, in a manner that is suitable to the rural, suburban, or urban context of the general plan. By requiring new duties of local officials, this bill would impose a state-mandated local program*”

Caltrans has also adopted Complete Streets with Deputy Directive 64-R1 in October 2008. As part of this directive Caltrans is actively implementing its Complete Streets policy in all planning, programming, design, construction, operations, and maintenance

activities and products on the State Highway System. Caltrans published the Complete Streets Implementation Action Plan to put this directive in motion. Implementing Complete Streets also supports California Global Warming Solutions Act of 2006 (AB 32) and Senate Bill 375.

Implementation of Complete Streets within the Specific Plan will encourage more walking by employees, visitors and local residents, which is consistent with Westlake Village residents' indicated desire for additional sidewalks and pedestrian amenities. Additionally, this would also encourage more bicycling which also is consistent with Westlake Village residents' indicated desire for additional bicycle facilities.

D. Existing Roadways

The master planned community of Westlake Village includes six arterials that were designed as the major means of vehicular travel to businesses, employment centers, residential neighborhoods and US Highway 101. Three of these important arterials serve the Specific Plan area: Thousand Oaks Boulevard, Lindero Canyon Road, and Via Colinas. Of these three arterials, Thousand Oaks Boulevard and Lindero Canyon Road are classified as Major Highways while Via Colinas is classified as a Secondary Highway. Also, Via Rocas, La Baya Drive and La Tienda Drive are Collector type roadways, and Corsa Avenue and Cedarvalley Drive function as Local type roadways. The street system for the Specific Plan area is shown in Figure 6-1.

The City of Westlake Village utilizes the roadway categories recognized by regional, state and federal transportation agencies. There are four categories in the roadway hierarchy, ranging from freeways with the highest capacity to two-lane undivided roadways with the lowest capacity. The roadway categories are summarized as follows:

- **Freeways** are limited-access and high-speed travel ways included in the state and federal highway systems. Their purpose is to carry regional through-traffic. Access is provided by interchanges with typical spacing of one mile or greater. No local access is provided to adjacent land uses.
- **Arterial** roadways are major streets that primarily serve through-traffic and provide access to abutting properties as a secondary function. Arterials are generally designed with two to six travel lanes and their major intersections are signalized. This roadway type is divided into two categories: principal and minor arterials. Principal arterials are typically four-or-more lane roadways and serve both local and regional through-traffic. Minor arterials are typically two-to-four lane streets that service local and commute traffic.

- Roadways within or adjacent to the Specific Plan area designated as principal arterials (i.e., Major Highway) include the following: Thousand Oaks Boulevard and Lindero Canyon Road.
- Roadways within the Specific Plan area designated as minor arterials (i.e., Secondary Highway) include the following: Via Colinas.
- **Collector** roadways are streets that provide access and traffic circulation within residential and non-residential (e.g., commercial and industrial) areas. Collector roadways connect local streets to arterials and are typically designed with two through travel lanes (i.e., one through travel lane in each direction) that may accommodate on-street parking. They may also provide access to abutting properties.
 - Roadways within the Specific Plan area that function as collector roadways include the following: Via Rocas, La Tienda Drive, and La Baya Drive.
- **Local** roadways distribute traffic within a neighborhood, or similar adjacent neighborhoods, and are not intended for use as a through-street or a link between higher capacity facilities such as collector or arterial roadways.
 - Roadways within the Specific Plan area that function as local commercial serving roadways include the following: Corsa Avenue and Cedarvalley Drive.

E. Circulation Improvements

Implementation of the Specific Plan will require improvements to the existing roadway system and transportation demand management measures. Descriptions of the planned regional improvements and circulation improvements to the roadways adjacent to and within the Specific Plan are provided in the following subsections.

1. Specific Plan Circulation Improvements

The recommended circulation improvements for the Specific Plan area build upon the roadway network systems that are currently in place with the focus on implementing complete streets. The recommended Specific Plan circulation improvements are intended to address changing the traffic patterns associated with build-out of the Specific Plan while fostering all travelers including motorists, bicyclists, public transportation users, and pedestrians of all ages and abilities. Summaries of the recommended Specific Plan circulation improvements are provided below for each roadway in the area, followed by detailed descriptions and cross sections of each roadway.

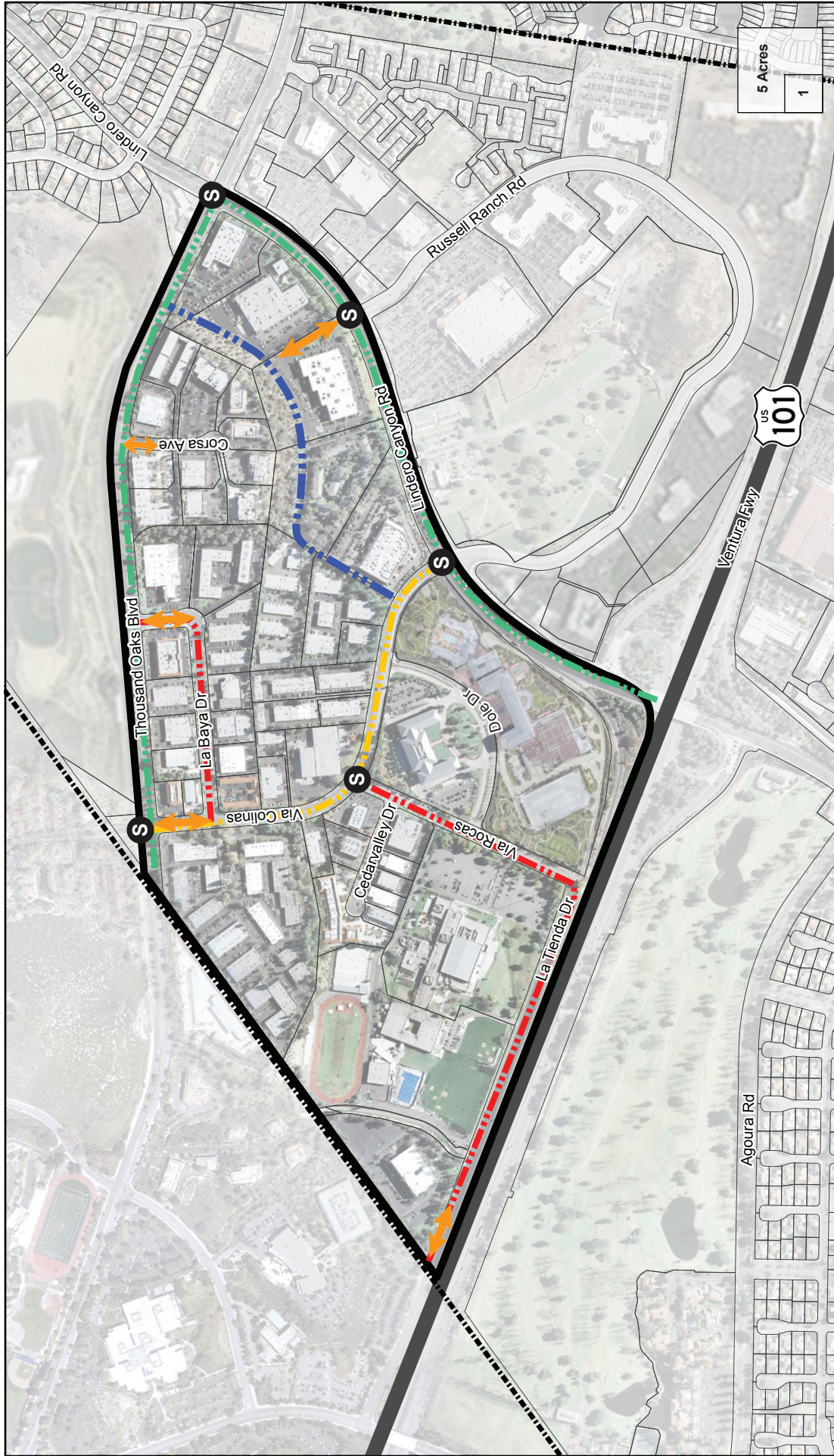


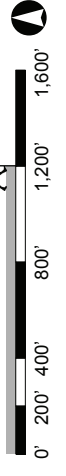
Figure 6-1:

Roadway Network

- Specific Plan Area
- City Boundary
- Major Hwy
- Secondary Hwy
- Collector
- Potential Private Access Drive
- S Traffic Signal
- ↔ Major Specific Plan Access Points

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



- **Thousand Oaks Boulevard:**
 - Installation of sidewalks along both sides of the roadway.
- **Lindero Canyon Road:**
 - Installation of a sidewalk along the west side of the roadway between the terminus of the existing sidewalk and Via Colinas.
- **Via Colinas:**
 - Restripe the existing roadway to accommodate two travel lanes in each direction for the segment between Lindero Canyon Road and Via Rocas (via removal of on-street parking).
 - Installation of Class II bicycle lanes in each direction.
 - Installation of sidewalks along both sides of the roadway.
- **Via Rocas:**
 - Installation of Class II bicycle lanes in each direction.
 - Installation of sidewalks along both sides of the roadway.
- **La Tienda Drive:**
 - Installation of Class II bicycle lanes in each direction.
 - Installation of a sidewalk along the north side of the roadway and a paved buffer on the south side of the roadway.
- **La Baya Drive:**
 - Installation of Class II bicycle lanes in each direction.
 - Installation of sidewalks along both sides of the roadway.
- **Corsa Avenue:**
 - Installation of sidewalks along both sides of the roadway.
- **Cedarvalley Drive:**
 - Installation of sidewalks along both sides of the roadway.

Following are detailed roadway classifications within the Specific Plan area and the planned cross-sections of the individual roadways.

Thousand Oaks Boulevard

Thousand Oaks Boulevard is an east-west oriented roadway that borders the Specific Plan to the north. Thousand Oaks Boulevard is designated as a Major Highway in the City of Westlake Village's Circulation section of the General Plan. Major highways are designed to carry high traffic volumes and provide connections between population and employment centers. Two through travel lanes are provided in each direction on Thousand Oaks Boulevard in the Specific Plan area. Exclusive left-turn lanes are provided in both directions at the intersections near the Specific Plan. Thousand Oaks Boulevard is posted for a speed limit of 45 miles per hour in the project area. Features of the Thousand Oaks Boulevard cross-section for the segment adjacent to the Specific Plan include:

- 100-foot Right-of-Way
- 84-foot roadway, curb-to-curb, that contains:
 - Four travel lanes (i.e., one 11-foot lane and one 11.5-foot lane in each direction)
 - 14-foot wide raised median island
 - Two 5-foot Class II bike lanes (one in each direction)
 - Two 7.5-foot shoulders within the roadway (one in each direction)
- 8-foot sidewalk/parkway widths

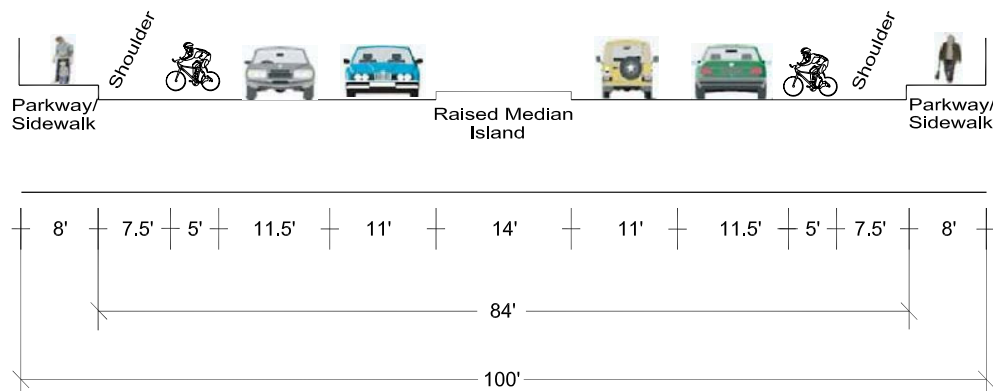


Figure 6-2: Thousand Oaks Boulevard

Lindero Canyon Road

Lindero Canyon Road is oriented northeast-to-southwest and borders the Specific Plan to the southeast. Lindero Canyon Road is designated as a Major Highway in the City of Westlake Village's Circulation section of the General Plan. Major highways are designed to carry high traffic volumes and provide connections between population and employment centers. Three through travel lanes are provided in each direction on Lindero Canyon Road in the Specific Plan area. Exclusive left-turn lanes are provided in both directions at the intersections near the Specific Plan site. Lindero Canyon Road is posted for a speed limit of 45 miles per hour in the project area. Features of the Lindero Canyon Road cross-section for the segment adjacent to the Specific Plan include:

- 100-foot Right-of-Way
- 84-foot roadway, curb-to-curb, that contains:
 - Six travel lanes (i.e., two 11-foot lanes and one 13-foot lane in each direction)
 - 14-foot wide raised median island
- 6-foot sidewalk along the west side of the roadway with a 2-foot retaining wall from Thousand Oaks Boulevard to the first southerly driveway
- One 4-foot meandering Class I bike-pedestrian facility along the east side of the roadway

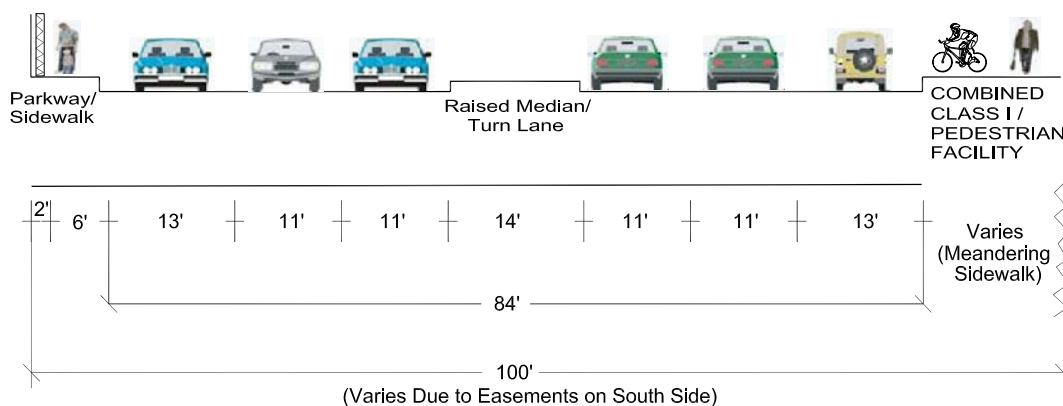
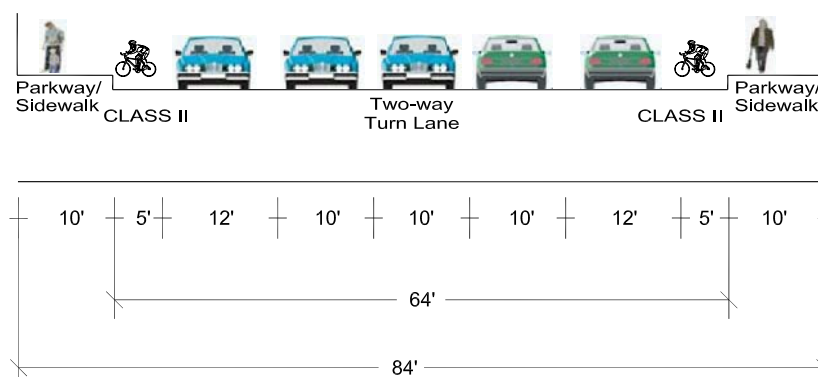


Figure 6-3: Lindero Canyon Road

Via Colinas (between Lindero Canyon Road and Via Rocas)

Via Colinas is a northwest-to-southeast oriented roadway and bisects the Specific Plan area. Via Colinas is designated as a Secondary Highway in the City of Westlake Village's Circulation section of the General Plan. Secondary highways represent the smallest of the arterial highway classifications. In the case of Via Colinas, the roadway serves businesses located within the Specific Plan and provides a direct connection between two primary arterials, Thousand Oaks Boulevard and Lindero Canyon Road, as well as to the U.S. 101 Freeway. One through travel lane is provided in each direction on Via Colinas in the Specific Plan area. Exclusive left-turn lanes are provided in both directions at the intersections near the Specific Plan area. Features of the Via Colinas cross-section for the segment between Lindero Canyon Road and Via Rocas within the Specific Plan include:

- 84-foot Right-of-Way
- 64-foot roadway, curb-to-curb, that contains:
 - Four travel lanes (i.e., one 10-foot and one 12-foot lane in each direction)
 - 10-foot two-way left-turn lane
 - Two 5-foot Class II bike lanes (one in each direction). It is recognized that the Class II bike lane installation may involve fairly significant roadway reconstruction and possibly right-of-way acquisition at the Lindero Canyon Road intersection.
- 10-foot sidewalk/parkway widths

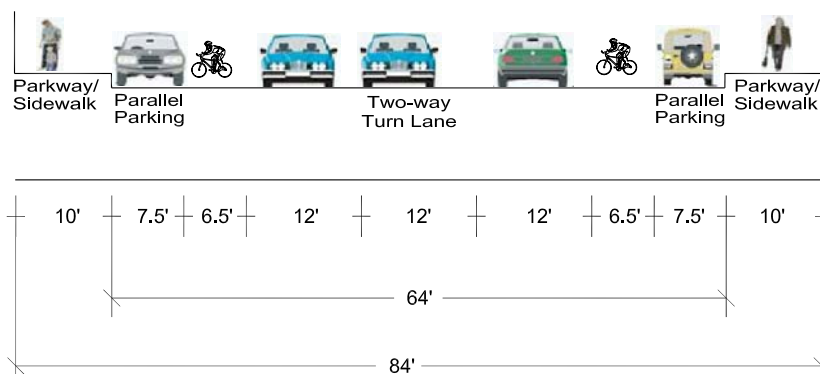


**Figure 6-4: Via Colinas
(between Lindero Canyon Road and Via Rocas)**

Via Colinas (between Via Rocas and Thousand Oaks Boulevard)

The section of Via Colinas between Via Rocas and Thousand Oaks Boulevard is recommended for one lane in each direction, bike lanes, parallel parking, as well as a two-way left turn lane. Features of the Via Colinas cross-section for the segment between Lindero Canyon Road and Via Rocas within the Specific Plan include:

- 84-foot Right-of-Way
- 64-foot roadway, curb-to-curb, that contains:
 - Two travel lanes (i.e., one 12-foot lane in each direction)
 - 12-foot two-way left-turn lane
 - Two 6.5-foot Class II bike lanes (one in each direction).
- 10-foot sidewalk along both sides
- Two 7.5-foot parallel parking lanes (one along each side of the roadway)



**Figure 6-5: Via Colinas
(between Via Rocas and Thousand Oaks Boulevard)**

Via Rocas

Via Rocas is a north-south oriented roadway in the Specific Plan area and extends between Via Colinas and La Tienda Drive. Via Rocas is designated as a Collector type roadway in the City of Westlake Village’s Circulation section of the General Plan. Collector roadways connect local streets to secondary or major highways. One through travel lane is provided in each direction on Via Rocas along with a two-way left-turn lane in the Specific Plan area. Features of the Via Rocas cross-section for the segment within the Specific Plan include:

- 84-foot Right-of-Way
- 64-foot roadway, curb-to-curb, that contains:
 - Two 12-foot travel lanes (one in each direction)
 - 12-foot two-way left-turn lane
 - Two 6.5-foot Class II bike lanes (one in each direction)
 - Two 7.5-foot parallel parking lanes (one along each side of the roadway)
- 10-foot sidewalk/parkway width

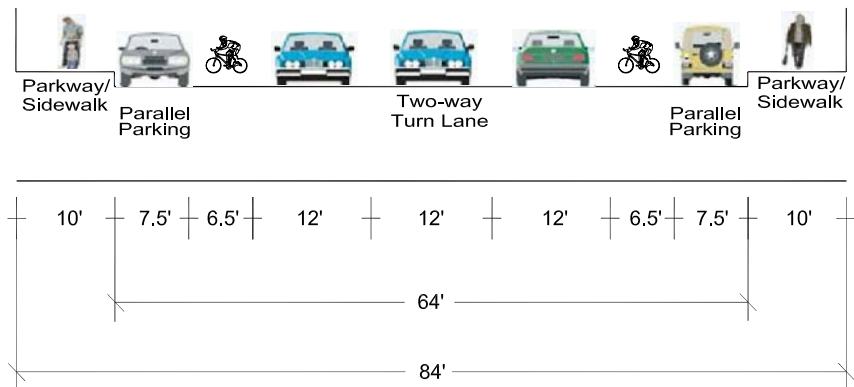


Figure 6-6: Via Rocas

La Tienda Drive

La Tienda Drive is an east-west oriented roadway in the Specific Plan area and extends between Via Rocas and Lakeview Canyon Road. La Tienda Drive is designated as a Collector type roadway in the City of Westlake Village's Circulation section of the General Plan. La Tienda Drive accommodates access to the institutional uses located along the north side of the roadway. One through travel lane is provided in each direction on La Tienda Drive along with a two-way left-turn lane in the Specific Plan area. Features of the La Tienda Drive cross-section for the segment within the Specific Plan include:

- 77-foot Right-of-Way
- 64-foot roadway, curb-to-curb, that contains:
 - Two 12-foot travel lanes (one in each direction)
 - 12-foot two-way left-turn lane
 - Two 6.5-foot Class II bike lanes (one in each direction)
 - Two 7.5-foot parallel parking lanes (one along each side of the roadway).
- 10-foot sidewalk/parkway along the north side of the roadway and a 3-foot paved buffer along the south side of the roadway

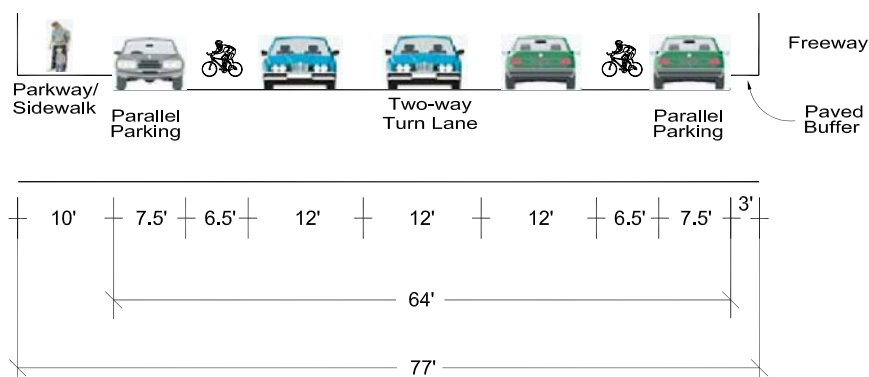


Figure 6-7: La Tienda Drive

La Baya Drive

La Baya Drive is primarily an east-west oriented roadway in the Specific Plan area and extends between Via Colinas and Thousand Oaks Boulevard. One through travel lane is provided in each direction on La Baya Drive along with a two-way left-turn lane in the Specific Plan area. Features of the La Baya Drive cross-section for the segment within the Specific Plan include:

- 84-foot Right-of-Way
- 64-foot roadway, curb-to-curb, that contains:
 - Two 12-foot travel lanes (one in each direction)
 - 12-foot two-way left-turn lane
 - Two 6.5-foot Class II bike lanes (one in each direction)
 - Two 7.5-foot parallel parking lanes (one along each side of the roadway)
- 10-foot sidewalk/parkway widths

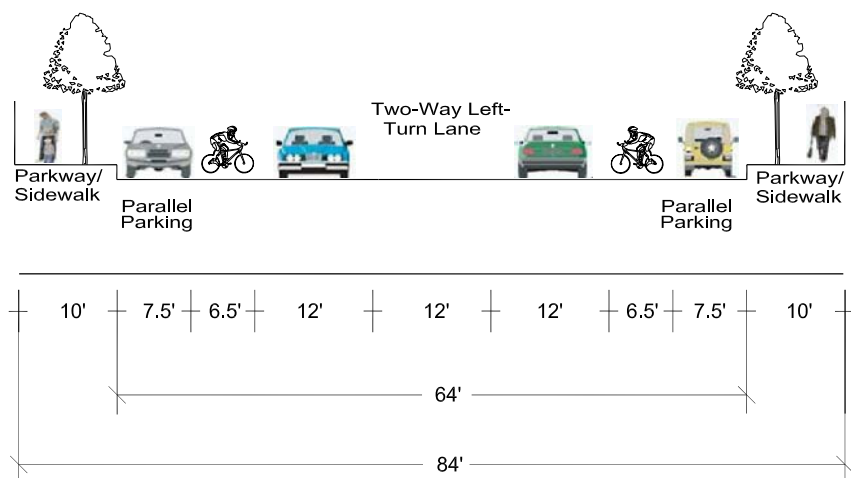


Figure 6-8: La Baya Drive

Private Access Drive

There is potential for a private access drive that would be a discontinuous roadway that extends between Thousand Oaks Boulevard and Via Colinas through the eastern portion of the Specific Plan area (see Figure 6-1). The intent of this private access drive would be to provide additional access options for motorists traveling to and from these areas of the Specific Plan. One narrow travel lane in each direction would be provided in a design similar to an enhanced alleyway. Reciprocal access easements between the property owners would be needed to facilitate implementation of this potential private access drive. It is noted that this roadway has not been assumed as part of the Specific Plan roadway network for evaluation purposes, as it would need to be implemented by individual property owners.

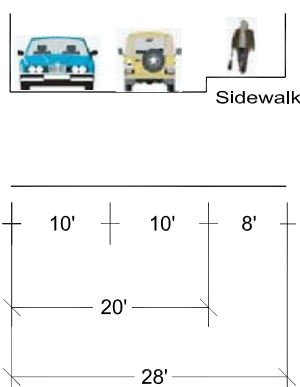


Figure 6-9: Potential Private Access Drive

2. Traffic Council

It is recommended that a Traffic Council be organized to identify and address mutual goals relating primarily to traffic, transportation and parking. The Traffic Council would operate in a similar manner to a homeowners association or neighborhood council that would represent all of the stakeholders within the Specific Plan area (as well as outside the Specific Plan area if the Traffic Council authority extends beyond the Specific Plan area). This would require membership and participation must be open to all stakeholders such as property owners, business owners, and tenants. The Traffic Council could be organized in any of the following ways:

- An official advisory body appointed by the City Council;
- An informal group of major employers and property owners that is encouraged to work together and supported by the City Council;

- An affiliate organization to the Chamber of Commerce in which the Chamber oversees the organization and management of the Traffic Council, including participation and direction for activities

Goals of the Traffic Council could include, but not be limited to, monitoring of traffic service levels and congestion on key roadways serving the Specific Plan area, coordinating and implementing measures such as staggered work schedules to relieve congestion, exploring and implementing shared parking opportunities to better manage parking resources, development of transportation demand management opportunities to reduce vehicular traffic, etc. In addition, the Traffic Council could be the appropriate organization to implement and oversee a Transportation Management Organization.

3. Transportation Management Organization

It is recommended that a Transportation Demand Management Organization (TMO) be formed to educate people about, and encourage Specific Plan area employees, visitors and residents to use alternative methods of travel than driving alone. The Specific Plan TMO could be a private sector, non-profit organization formed to address, coordinate and help implement cost effective transportation demand management programs to ease traffic congestion, meet clean air requirements and improve access to, from and within the Specific Plan. The TMO would work in concert with the Chamber of Commerce and/or a local Business Improvement District (BID) to promote the best interests of the Specific Plan area and its membership, serving as a primary organizer for business development and civic growth through effective communication, events and programs making the Specific Plan a preferred place to work, visit, live and shop within the City of Westlake Village.

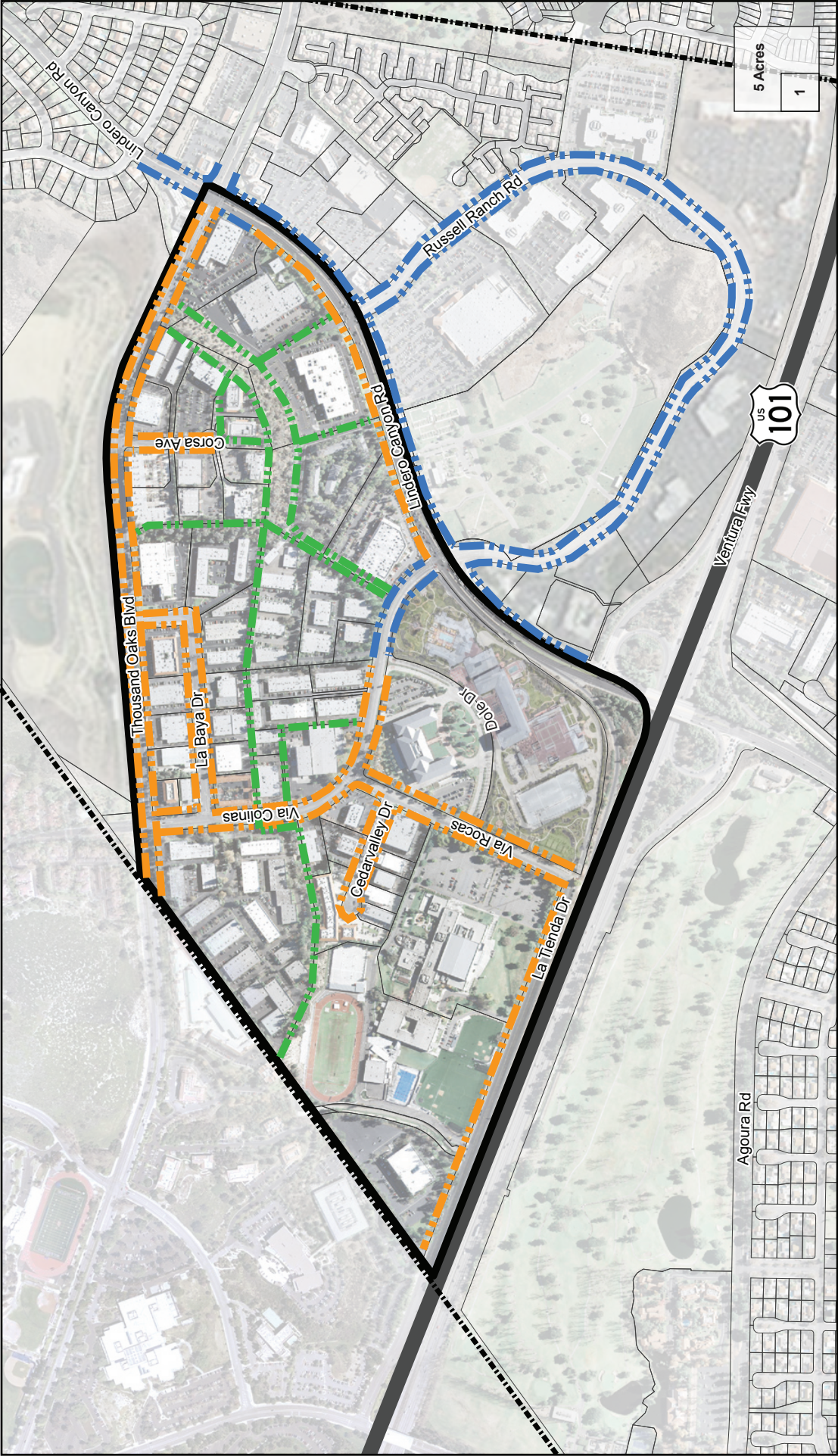
F. Pedestrian Circulation

Improved pedestrian circulation is a major goal of the Specific Plan, and the North Business Park area is well-positioned to facilitate increased pedestrian activity. This major goal is to create an environment where people can walk to various activity points within the Specific Plan area. Pedestrian paths and connections, along with plazas and other open spaces, are used to integrate the districts in the Specific Plan and knit the area together with the surrounding community fabric. The sidewalks, paths and pedestrian connections are planned to allow people to accomplish local trips without driving, and are expected to contribute towards a human-scale and sense of community.

The pedestrian circulation plan of this Specific Plan has been designed to encourage pedestrian activity and walking as a transportation mode, and to interconnect the districts on a pedestrian level. Pedestrian sidewalks and pathways are planned throughout the Specific Plan area, along with connections to the adjoining commercial and residential areas, in a manner that promotes walkability (walkability is a term for the extent to which walking is readily available as a safe, connected, accessible and pleasant mode of transport). There are five basic components that are widely accepted as the key to achieving walkability, with the underlying principle being that pedestrians should not be delayed, diverted, or placed in danger. The five primary components of walkability include the following:

- **Connectivity:** People can walk from one place to another without encountering major obstacles, obstructions, or loss of interconnection.
- **Convivial:** Pedestrian routes are friendly and attractive, and are perceived as such by pedestrians.
- **Conspicuous:** Suitable levels of lighting and visibility over its entire length, with high quality delineation and signage.
- **Comfortable:** High quality and well-maintained footpaths of suitable widths, attractive landscaping and architecture, shelter and rest spaces, and a suitable allocation of road space to pedestrians.
- **Convenient:** Walking is a realistic travel choice, partly because of the impact of the other criteria set forth above, but also because walking routes are of a suitable length as a result of land use planning with minimal delays.

These five primary characteristics will be accommodated under the recommended Specific Plan pedestrian improvements as shown in Figure 6-10. The pedestrian network will provide connectivity throughout the Specific Plan area, with the adjacent residential neighborhood and commercial areas, as well as to transit stops. In particular, the internal pedestrian pathways have been aligned to account for the topography of the northerly and central portions of the Specific Plan as well as to provide connections between the Specific Plan zoning districts. The pedestrian walkways within the Specific Plan area will be appropriately landscaped and adorned to provide a friendly walking environment (refer to Chapter 7, Open Space and Streetscape Improvements).



- Specific Plan Area
- City Boundary
- Existing Sidewalk
- New Sidewalk
- New Pedestrian Path

Figure 6-10:

Pedestrian Network

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



G. Bicycle Circulation

The Federal and State transportation system recognizes three primary bikeway facilities: Bicycle Paths (Class I), Bicycle Lanes (Class II), and Bicycle Routes (Class III). Bicycle Paths (Class I) are exclusive car free facilities that are typically not located within a roadway area. Bicycle Lanes (Class II) are part of the street design that is dedicated only for bicycles and identified by a striped lane separating vehicle lanes from bicycle lanes. Bicycle Routes (Class III) are typically located on collector and lower volume arterial streets. The City of Westlake Village bicycle network currently includes Class II bicycle lanes on Thousand Oaks Boulevard and a Class I combination bicycle-pedestrian path along the east side of Lindero Canyon Road.

Bicycle access throughout the area will be facilitated by the existing City of Westlake Village bicycle roadway network along with enhancements to the interior Specific Plan roadways. A major goal of the Specific Plan is to enhance the City's bicycle roadway network to encourage bicycle activity and bicycling as a transportation mode both on a local and area-wide basis. Class II bicycle lanes are recommended throughout the Specific Plan as follows:

- **Via Colinas:** Class II bicycle lanes between Thousand Oaks Boulevard and Lindero Canyon Road connecting to the existing Class II and Class I bicycle facilities, respectively.
- **Via Rocas:** Class II bicycle lanes between Via Colinas and La Tienda Drive.
- **La Tienda Drive:** Class II bicycle lanes between Via Rocas to Lakeview Canyon Road (just west of the Specific Plan). It should be noted that a portion of this roadway segment is located within the City of Thousand Oaks and would require its support for full implementation.
- **La Baya Drive:** Class II bicycle lanes between Via Colinas and Thousand Oaks Boulevard

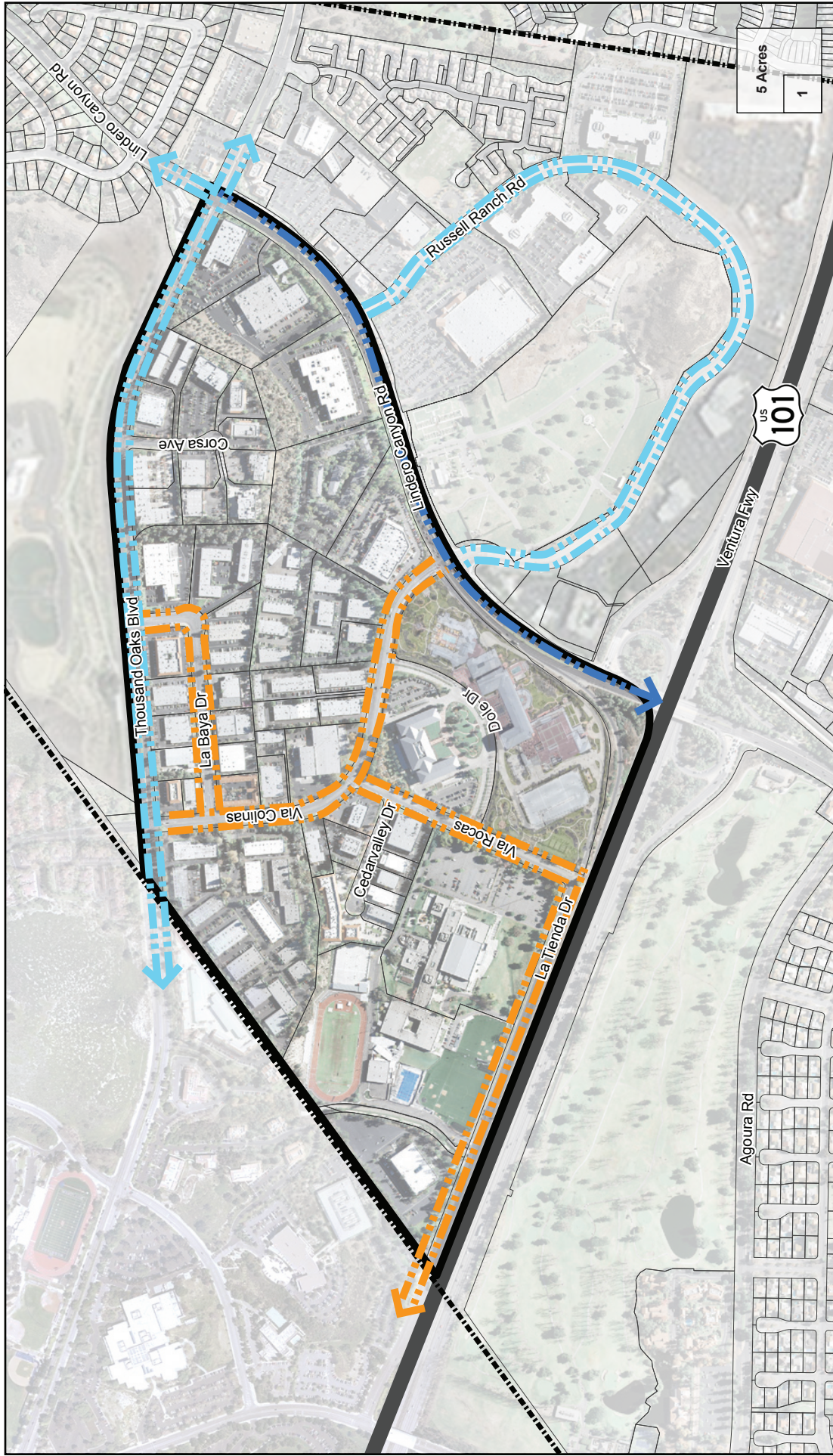


Figure 6-11:

Bicycle Network

- Specific Plan Area
- City Boundary
- Existing Class I Bicycle Facility (Separated)
- Existing Class II Bicycle Facility (Lane)
- New Class II Bicycle Facility (Lane)

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



The existing and Specific Plan area bicycle network is illustrated in Figure 6-11.

H. Transit

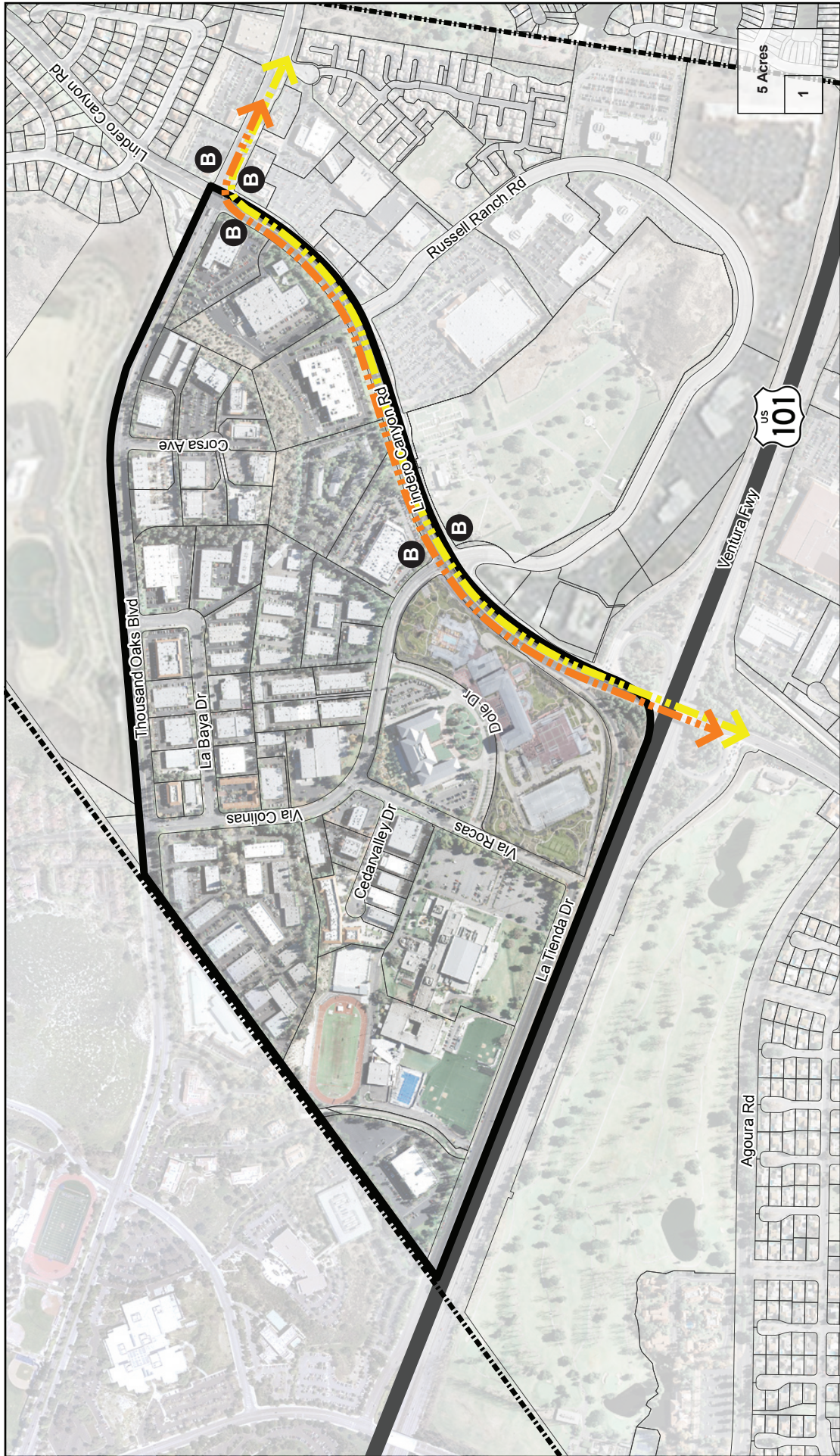
Public bus transit service within the Specific Plan area is currently provided by Westlake Village Transit, Thousand Oaks Transit (TOT), Los Angeles Metropolitan Transportation Authority (Metro), and City of Los Angeles Department of Transportation (LADOT Commuter Express). Also, regional rail service is provided both by Metrolink and Amtrak with the nearest station located in Moorpark near Route 118 Freeway. The existing public transit routes provided within the Specific Plan area are illustrated in Figure 6-12. A summary of the existing transit service provided within the Specific Plan area is provided below:

- There are a limited number of commuter bus transit routes provided near the Specific Plan area (i.e., only along Lindero Canyon Road), and none traverse the Specific Plan area.
- The City of Westlake Village operates a local trolley that connects the business park, Shoppes, community park, and areas south of U.S. 101 Freeway. The service is only provided from May 25th through September 1st and consists of one vehicle making one circuit roughly every 50 minutes from 3:00 PM to 10:00 PM.

In the future, it is recommended that consideration be given to implementing a local community shuttle service that would connect the Specific Plan area, the community park, nearby commercial and residential areas, as well as key points south of U.S. 101 Freeway. As build-out of the Specific Plan progresses and transit becomes increasingly vital in serving that growth, it is expected that measures will need to be taken to ensure that transit is a viable alternative to the automobile. These measures may include implementation of a local community shuttle service, facilitating bus movements into and through the Specific Plan, and accommodating at least equal priority of transit on roadways within and adjacent to the Specific Plan by strategically aligning routes and provision of bus stops.

I. Trucks

There is no change planned for truck access to and from the Specific Plan area. It is anticipated that truck movements associated with service and delivery of goods will continue to utilize U.S. 101 Ventura Freeway and the major arterials (i.e., Thousand



- Specific Plan Area
- City Boundary
- Bus Stop
- La Metro Route 422/423
- La Metro Route 161

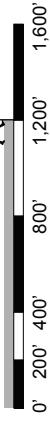
The City of Westlake Village also operates "The Village Trolley" during the summer months (May 25th through September 1st). The trolley loop service is provided on Fridays and Saturdays between 3:00 - 10:00 PM with entertainment services from 6:00 - 8:00 PM. Each loop has a duration of 50 minutes. A total of four stops are located within or directly adjacent to the Specific Plan area.

Figure 6-12:

Transit Network

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



Oaks Boulevard and Lindero Canyon Road) on the periphery of the Specific Plan, as well as the roadways located within the Specific Plan.

J. Specific Plan Parking

This section identifies recommendations for Specific Plan parking to meet future demand with build-out of the area. Specific vehicle and bicycle parking ratios are discussed in Chapter 4. Also, this section provides an overview for potential shared parking opportunities and parking strategies, including general approaches to the potential establishment of a parking district within the Specific Plan area.

1. Context

The North Business Park Specific Plan area is essentially built-out with a wide variety of commercial land uses, including auto repair, distribution, general office, hotel, light industrial, and warehouse type uses. A total of 54 parcels exist within the Specific Plan with the bulk of the parcels being less than two acres in size. It should be noted that there are multiple owners of these parcels, including commercial condominium owners.

Surface parking lots are provided within most of the 54 individual parcels that comprise the Specific Plan. Based on data provided by the City of Westlake Village, a total of approximately 4,562 off-street parking spaces is provided within the surface lots associated with these existing parcels. Some sharing of parking lots is accommodated on those parcels with multi-uses and/or multiple owners. It is assumed that reciprocal agreements on parking and access have been executed for these parcels. There are also three parking structures within the Specific Plan area: one at the Four Seasons Hotel, one underground structure at Dole Headquarters, and one at Oaks Christian School.

On-street parking is allowed along roadways such as Via Colinas (i.e., east side of the street near La Baya Drive), La Baya Drive, Cedarvalley Drive, Via Rocas, La Tienda Drive and Corsa Avenue. For those roadways where on-street parking is allowed, parking utilization appears to be fairly high. Additionally, it should be noted that on-street parking is not allowed on the periphery of the Specific Plan along Thousand Oaks Boulevard and Lindero Canyon Road.

Some of the key issues and goals for the Specific Plan are as follows:

- Design parking in convenient and accessible locations to future development;
- Strategically locate parking such that multiple uses can share parking, as different land uses require parking at different times of the day, days of the week; and

- Consider implementation of a parking district which allows people to park once and travel conveniently within the Specific Plan area.

Parking to meet projected demand will be accomplished using a combination of surface and structured parking. Shared parking arrangements are appropriate for the Specific Plan development program due to varying peak demands among the office, retail, restaurant, and entertainment uses.

2. Specific Plan Parking Requirements

The parking requirements for the Specific Plan were established to meet future demand with build-out of the area. However, the goal was to establish minimum parking requirements appropriate for the Specific Plan without providing unneeded parking that wastes space and money.¹ The parking requirements for the Specific Plan were established based on consideration of the following items:

- The mix of complementary land uses planned for the Specific Plan;
- The major goals of the Specific Plan related to creating a sustainable environment by promoting a walkable environment, bicycling, and interconnections throughout the area;
- Existing parking requirements identified in Article 9 (Zoning Regulations) of the Westlake Village Municipal Code, Chapter 9.19 (Off-Street Parking and Loading Standards); and
- Data provided in the following publications:
 - *Parking Generation*, 4th Edition, 2010, Institute of Transportation Engineers (ITE)
 - *Shared Parking*, 2nd Edition, 2005, Urban Land Institute (ULI)

The number of vehicular off-street parking spaces required for each of the identified Specific Plan land uses is provided in Chapter 4. Parking requirements may be reduced after a detailed parking management plan has been designed and approved by the Planning Director and City Engineer. For mixed-use or non-residential development, a parking study shall be required by an applicant aiming to reduce their minimum project parking requirement. The study shall propose the use of parking alternatives for non-residential uses, such as valet, shared parking, and/or on-street parking. In addition, close proximity to a transit stop (1/4 mile) could also be used as means to

¹ United States EPA: “[T]ypical parking regulations and codes simply require a set amount of parking for a given square footage or number of units, assuming all trips will be by private automobile and ignoring the neighborhood’s mix of uses, access to transit and walking, and context within the metropolitan region. Such inflexible parking requirements can force businesses to provide unneeded parking that wastes space and money....Inflexible minimum parking requirements are the norm – but they represent a barrier to better development.” (EPA, 2006).

justify the reduction in minimum parking requirements for non-residential uses. It is recommended that a reduction of no more than 25% of the total parking spaces required for non-residential uses be granted absent the City adopting lower parking ratios at some point in the future. If changes to the uses occur at some future time, an updated parking study will be required to reflect the new uses.

ITE's *Parking Generation* manual provides data on actual parking demand for a wide variety of land uses. The manual is based on a national database of parking demand studies. The parking demand studies that ITE uses as a basis for the manual are primarily stand-alone, suburban developments where all parking is provided on-site and is free to the user.

ULI's *Shared Parking* manual provides recommended peak-parking demand rates, but then modifies each land use by key factors such time of year, week, and day. The ULI shared parking methodology also allows for adjustments with "mode adjustment" and "non-captive ratio". Mode adjustment is a variable based on the percentage of trips that are made to the site using vehicles. Non-captive ratio is an estimate of the percentage of motorists parking at a site in a mixed or multi-use development who are not already counted as being parked at another of the land uses. Accordingly, this ratio accounts for multiple trip making in terms of parking which essentially is the definition of shared parking (i.e., one parking space is utilized by a person while visiting multiple land uses).

3. Potential for Shared Parking

The concept of shared parking is widely recognized within the transportation planning industry and accounts for the changes in parking demand over time for different types of land uses within an individual project site, or within development areas such as the Specific Plan. The shared parking concept incorporates the analysis procedures recommended in the *Shared Parking* manual published by ULI, and is consistent with the methodology used by the City of Westlake Village in the review and approval of shared parking applications for various projects. The Shared Parking manual provides recommendations with respect to the following characteristics of parking demand:

- **Hourly Parking Indices.** The *Shared Parking* manual provides hourly parking indices for various land uses. For example, the hourly parking demand for retail (which generates its peak parking demand during the early afternoon period) is different than the parking demand associated with a restaurant (which generates its peak parking demand concentrated around mid-day lunch hour).

- **Day of Week Parking Variations.** The *Shared Parking* manual provides recommendations for day of week parking factors. For example, office uses experience their peak parking demands during weekdays but experience minimal demand during weekends. Retail uses, for example, generally have a higher demand for parking during weekends as compared to weekdays.

In sum, the shared parking methodology defines the ability to share parking spaces as the result of two conditions: variations in the accumulation of vehicles by hour, by day, or by season at the individual land uses and relationships between the land uses that result in employees, visitors and patrons visiting multiple land uses during the same vehicle trip.

The concept of shared parking where there are mixed land uses has been an accepted practice in most jurisdictions including suburban type cities such as the City of Westlake Village. Potential shared parking opportunities are anticipated for land uses on individual parcels and within the separate districts included in the Specific Plan.

4. General Parking District Approaches

The ability to provide adequate on-site parking often is an important factor limiting the redevelopment potential of individual private parcels. To be economically viable, redevelopment generally requires either additional land use on a site, or an intensification of existing land use quantities, both of which typically increase parking needs. Challenges associated with limited parcel size, code parking requirements, and other building restrictions in some instances make on-site provision of increased parking difficult if not impossible.

Joint development of shared, public parking facilities via a Parking Improvement District may allow better shared use of parking spaces than does provision of on-site private parking. Public parking facilities accommodate the shared parking methodology and approach to providing parking spaces for complementary land uses. Moreover, off-site parking often times can have its greatest application in developed areas where small lots, multiple landowners, and physical constraints prevent the construction of on-site parking.² Parking Improvement Districts are designed to aid general economic development and to facilitate property owner, business owner and merchant cooperation. A parking improvement district is a local self-help funding mechanism that allows property owners and businesses within a defined area to establish a special assessment district. The assessment can be used to finance: construction, acquisition, and/or maintenance of parking facilities in the area; decoration of public areas; implementation of wayfinding programs; and the like.

² Flexible Parking Requirements, Thomas P. Smith, 1983.

5. Design District Parking Strategy

The Design District (North and South), which is bifurcated by La Baya Drive, is a candidate area within the Specific Plan for implementation of a Parking Improvement District. With the expectation that complementary land uses will be congregated in the Design District, there is the opportunity to consolidate parking in one location to foster the “park once strategy” (i.e., allow motorists to park once to visit multiple land uses) and encourage pedestrian activity by reducing the need for driveways and parking provided within the individual parcel. Also, as previously noted, off-site parking often times can have its greatest application in developed areas where small lots, multiple landowners, and physical constraints prevent the construction of on-site parking. This description is similar to the make-up of the existing parcels located in the Design District situated along La Baya Drive.

An in-lieu fee program could be utilized as the mechanism for the provision of parking, thereby reducing the need for variances. This approach would help to ensure that all landowners were treated equitably. In addition, an in-lieu fee program increases the feasibility of development, or redevelopment, particularly for small lots thereby allowing property owners and developers to address the often-difficult issue of meeting parking requirements. However, it is noted that sufficient funding needs to be available (either through the in-lieu program or from other sources) to ensure that parking is actually provided, particularly if the first few developments taking advantage of an in-lieu program are relatively small (and therefore would not generate funds sufficient to construct new parking). In this instance, some initial public funding could be required.

6. Effects of New Technologies on Parking Demand

A number of today’s new motor vehicles have technology that helps drivers avoid drifting into adjacent lanes or making unsafe lane changes, or that warns drivers of other vehicles behind them when they are backing up, or that brakes automatically if a vehicle ahead of them stops or slows suddenly, among other things. While this technology is aimed at increasing safety, it does not cause a dramatic shift in overall parking demand. The introduction of self-driving automobiles, however (i.e., which are still in the testing phase/s), likely will have an impact on future parking demand and parking needs. Vehicles with autonomous function will allow the former driver and any passenger occupants to be dropped off at their destination or at a designated drop-off/pick-up point. The self-driving vehicle would then be programmed to return to place of origin to park or at another designated destination. Over time, parking demands could very well decline in the future, causing many agencies and jurisdictions to reconsider and revisit their current parking requirements.

7. Bicycle Parking

Bicycle parking for employees, residents and patrons who bicycle to, from, and within the Specific Plan area must be provided and designed in such a way as to be attractive, safe, and convenient. Given the extensive existing and planned bikeways (paths, lanes, and routes) in and around the Specific Plan area, the goal is that bicycling can become a transportation mode of choice for many patrons within and of the Specific Plan area. Details as to the number of bicycle parking spaces required for commercial and residential land uses within the Specific Plan is provided in Chapter 4.

Open Space and Streetscape Improvements

A. Introduction

This chapter presents open space and streetscape design concepts for the North Business Park Specific Plan area. The recommended open space improvements are based on the high value the community places on open space, integrating green areas and plazas within commercial development, as well as the City's use of greenbelts in many of its residential neighborhoods. The planned landscape treatments draw from the presence of oak trees and other appropriate California native or adapted plants within the City to maintain visual continuity and create a sustainable plant palette. Overall, the existing and recommended open spaces along with the planned streetscape improvements, will encourage pedestrian movement throughout the Specific Plan area.

B. Open Space

With just over 1,300 acres of dedicated open space, Westlake Village is characterized by its beautiful natural setting and outdoor lifestyle. Open space policies in the City's General Plan are intended to ensure that adequate open space and parklands are maintained for existing and future residents in balance with new development, and to require that development be sited and designed to include the provision of open space. In addition, the preservation of significant ridgelines as a visual and open space resource is required.

The North Business Park and surrounding environment already contain a significant amount of passive and recreational open space, as identified in Figure 7-1. This includes the Westlake Village Community park just north of the Specific Plan area, the open space and recreational fields for Oaks Christian school, and the gardens and recreational open space for the Four Seasons Hotel. In addition, the Valley Oaks-Griffin Memorial Park cemetery is located just east of the North Business Park, contributing to the green, open setting of the area.

In keeping with the city’s goal to balance open space with new development, and to provide outdoor amenities for employees, visitors and future residents of the North Business Park, useable open space is encouraged throughout the Specific Plan area in the form of plazas, courtyards, and passive outdoor spaces as new development occurs. In the Mixed Use Corsa, Mixed Use Lindero, and Office zoning districts, where the most redevelopment is anticipated to occur, larger non-residential projects are required to provide common open space, as set forth in Chapter 4. In the Mixed Use Corsa zoning district, such open space could be in the form of a large “village green” that could provide outdoor amenities and also host activities and events.

The topography of the North Business Park offers beautiful views of the city and Santa Monica Mountains to the south. The open space framework takes advantage of these views by requiring new development in the Mixed Use Corsa and Business Park West zoning districts to locate open space along the ridgeline in the form of a greenbelt or linear park. The linear park shall be designed to take advantage of the views and provide passive open space for employees, customers, and residents. Features of the greenbelt/linear park may include a walkway, seating, small terrace, and appropriate landscaping to beautify the area.

A large plaza or “village green” in the Mixed Use Corsa zoning district could greatly enhance the area and provide amenities for employees, visitors and future residents, such as outdoor seating, walking paths, gardens, passive open space, as well as areas for small events or activities.



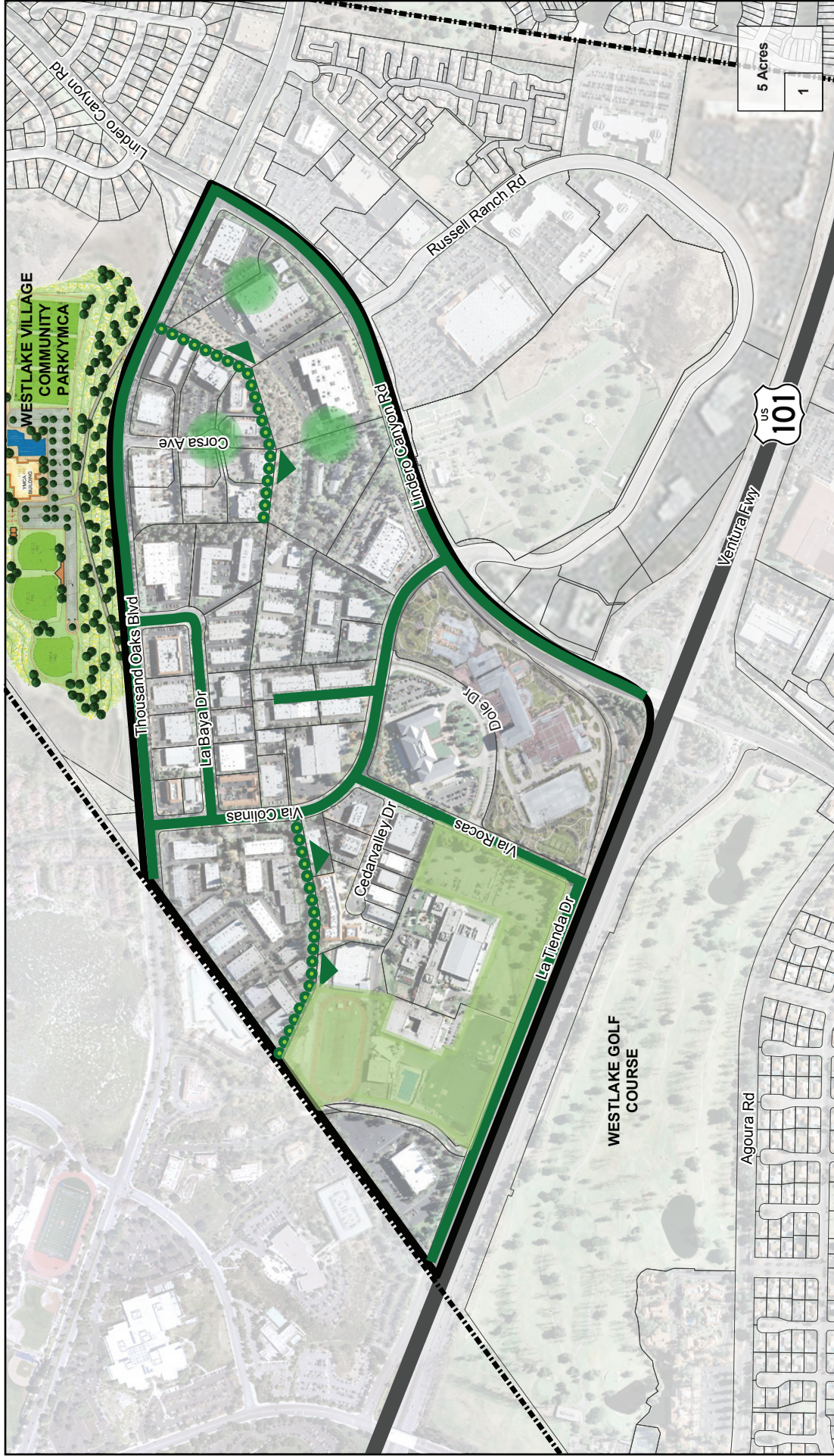


Figure 7-1:

Open Space Framework

School Open Space/ Playing Fields

Scenic Vistas

Specific Plan Area

City Boundary

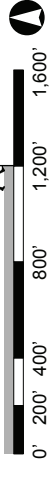
Upgraded Streetscape Improvements

Village Green/ Large Plaza (Conceptual Locations)

Linear Parks

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



C. Streetscape Improvements

The overall streetscape design intent is to provide a safe and comfortable pedestrian environment throughout the North Business Park. Streetscape improvements are meant to enhance and unify the visual and spatial experience of the driver, pedestrian, and bicyclist and help create key linkages between throughout the Specific Plan area. A comfortable experience through the streetscape environment gives the user a sense of direction and a sense of place.

Planting new street trees per a consistent street tree palette will help unify the North Business Park. A uniform pattern of street trees will also increase connectivity and sense of cohesion on a pedestrian and auto-oriented scale, as well as offer a pleasant sidewalk experience. In addition, new street trees will provide shade, add seasonal color, define the street edge, and add to the urban forest. Canopy trees will also lower overall temperatures on sidewalks and roadways by shading, as well as capture and treat storm water before it drains into the local watershed. In addition, by providing ample shade with street trees and converting existing plantings to more appropriate California native or adapted plants, water needed for irrigation can be greatly reduced.

Table 7-1 summarizes the designated street trees for the Specific Plan roadways. The street tree plan for the North Business Park draws from the **Westlake Village Street Tree Master Plan** to maintain visual unity throughout the community. Following Table 7-1 are descriptions and cross sections for the designated streetscape treatments for each of the Specific Plan streets.

Table 7-1: Recommended Street Trees for the North Business Park Specific Plan area

Street	Common Name	Botanical Name	Notes
Thousand Oaks Boulevard	Coastal Live Oak	<i>Quercus agrifolia</i>	Historical tree, low water use, provides shade and habitat
Thousand Oaks Boulevard (median)	London Plane Tree	<i>Platanus x acerifolia</i> 'Bloodgood'	Deciduous, low water use, resistant to anthracnose
Lindero Canyon Road	Canary Island Pine	<i>Pinus canariensis</i>	Evergreen, low water use
Via Colinas	Chinese Flame Tree	<i>Koelreuteria bipinnata</i>	Pedestrian-scaled, low water use, provides fall color, good for bicycle and walking paths
Via Rocas	Chinese Flame Tree	<i>Koelreuteria bipinnata</i>	Pedestrian-scaled, low water use, provides fall color, good for bicycle and walking paths
Cedarvalley Drive	Chinese Flame Tree	<i>Koelreuteria bipinnata</i>	Pedestrian-scaled, low water use, provides fall color, good for bicycle and walking paths
La Tienda Drive	Stone Pine	<i>Pinus pinea</i>	Evergreen, filters air from adjacent freeway, open for views
La Baya Drive	London Plane Tree	<i>Platanus x acerifolia</i> 'Bloodgood'	Deciduous, low water use, resistant to anthracnose



Chinese Flame Tree
(*Koelreuteria bipinnata*)



Eastern Redbud
(*Cercis canadensis*)



Coastal Live Oak
(*Quercus agrifolia*)



London Plane Tree
(*Platanus x acerifolia* 'Bloodgood')

Recommended street
trees for the North
Business Park Specific
Plan Area.



Stone Pine
(*Pinus pinea*)



Canary Island Pine
(*Pinus canariensis*)

Thousand Oaks Boulevard

Five-foot wide sidewalks along each side of Thousand Oaks Boulevard will be separated from the street with a 3-foot planted parkway and street trees planted every 40 feet. The existing, raised 14-foot wide median will be planted with large and small trees in a loose arrangement to give an informal character in context with the adjacent hillsides. Street lights approximately every 160 feet and benches and trash receptacles at each intersection provide a safe environment for pedestrians. The Coast Live Oak is recommended as the signature tree for Thousand Oaks Boulevard, complimented with London Plane Trees and Eastern Redbuds planted in the median.

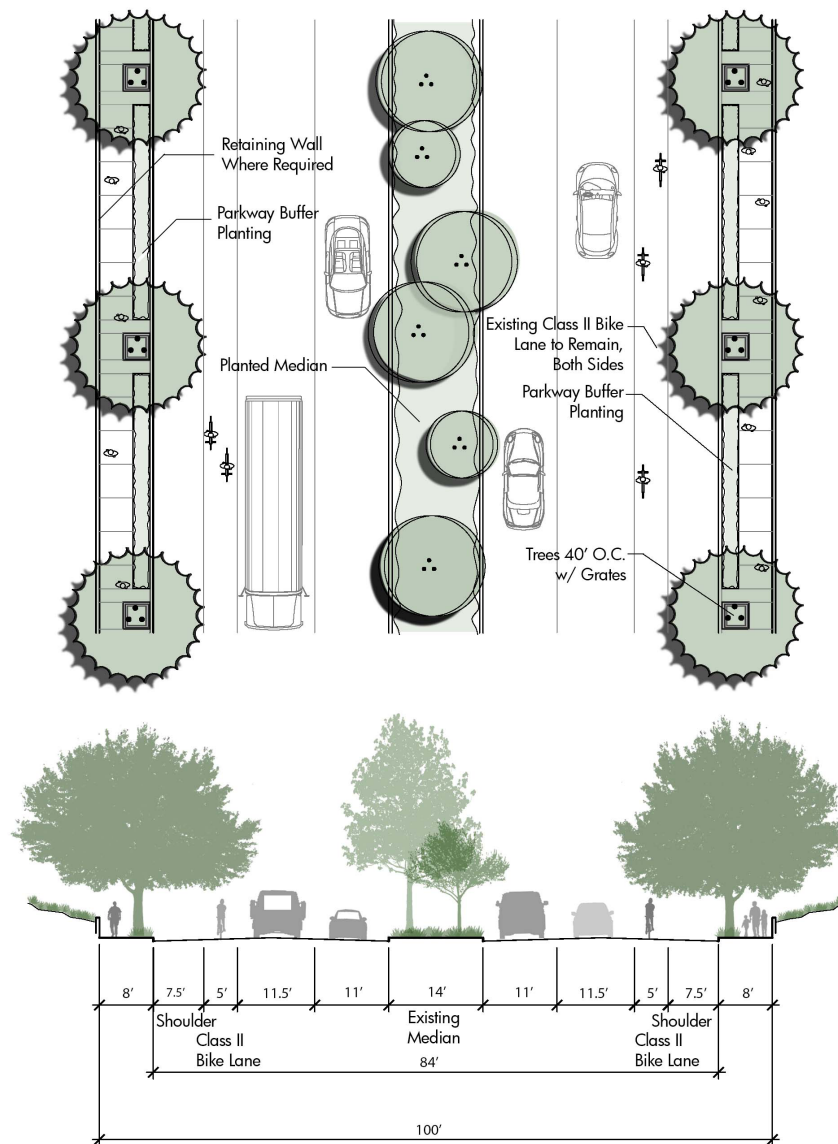


Figure 7-2: Thousand Oaks Boulevard

Lindero Canyon Road

Lindero Canyon has an existing median and sidewalk on the east side of the street that will remain. A new 6-foot wide sidewalk and 2-foot high retaining wall on the west side with added street lights approximately every 150 feet will provide a safe pedestrian environment. Benches and trash receptacles are recommended at an approximate spacing of 1,000 feet and at major intersections. Spacing may be adjusted to accommodate safe and convenient maintenance of trash receptacles. It is recommended that the privately-owned slope on the west side of Lindero Canyon Road be planted with a low maintenance evergreen species, such as the Canary Island Pine, which is the signature tree for Lindero Canyon Road in the Westlake Village Street Tree Master Plan.

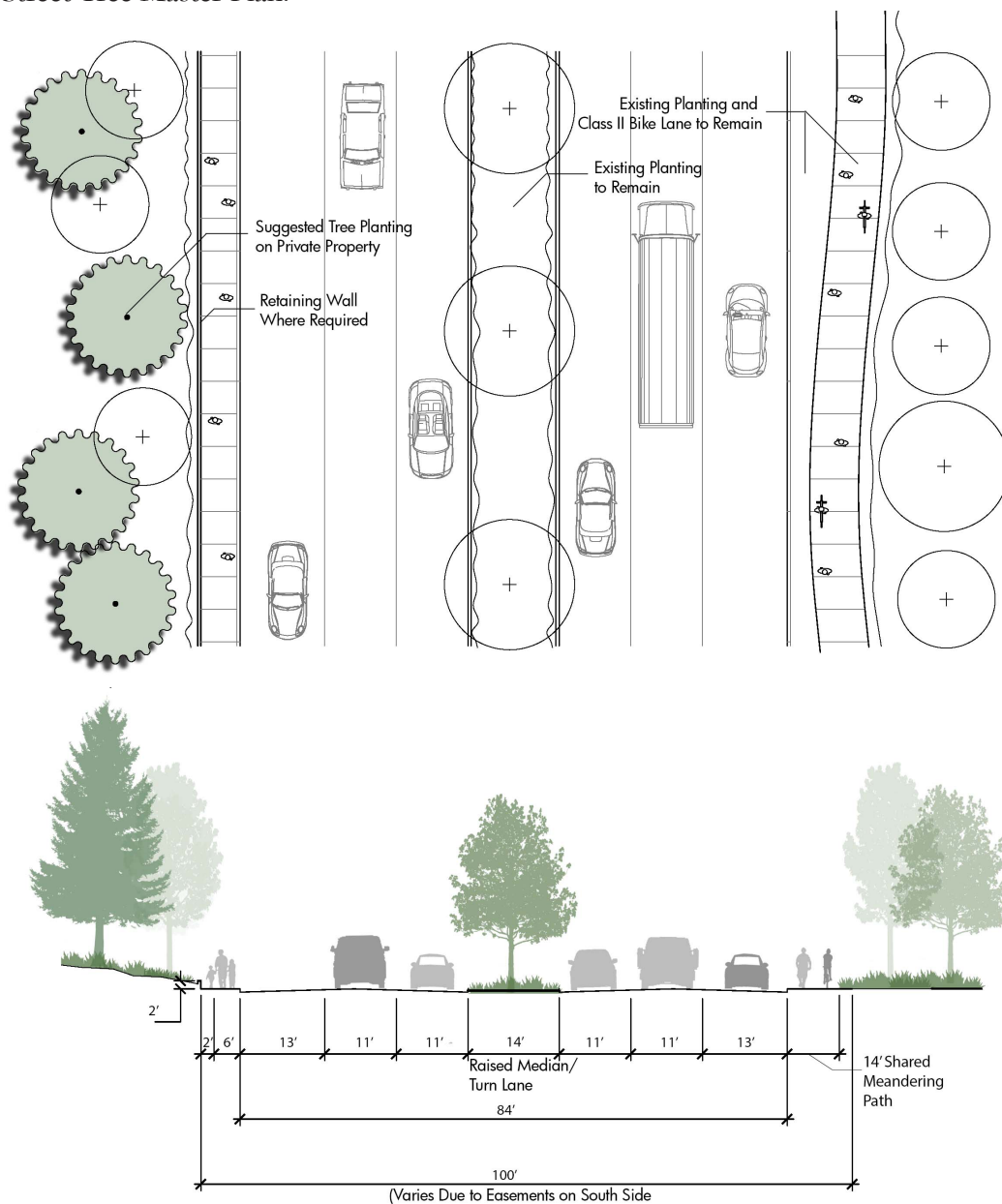
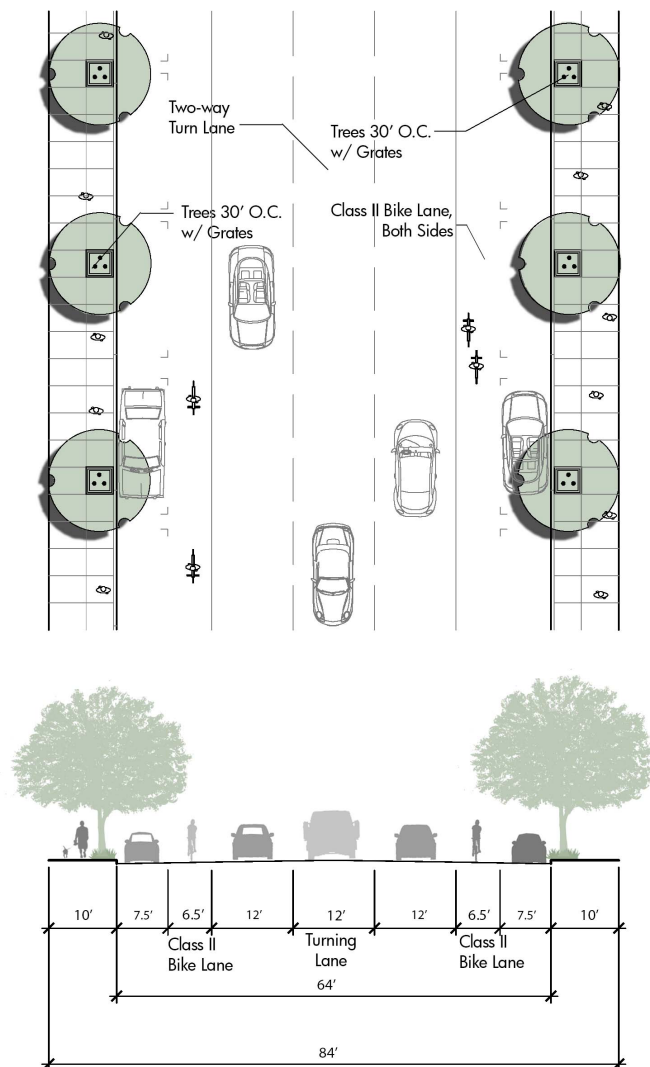


Figure 7-3: Lindero Canyon Road

Via Colinas (between Via Rocas and Thousand Oaks Boulevard)

Between Via Rocas and Thousand Oaks Boulevard, Via Colinas is recommended for one lane in each direction with a two-way center turn lane, parallel parking, and Class II bike lanes on each side. Both sides of the street have 10-foot wide sidewalks. On both sides of the street, trees planted every 30 feet and pedestrian streetlights approximately every 60 feet provide shade and lighting for a comfortable pedestrian environment. Benches and trash receptacles placed regularly will add convenience for pedestrians and cyclists. The recommended street tree for Via Colinas is the pedestrian-scaled Chinese Flame Tree, which will provide seasonal change, color, and shade.



**Figure 7-4: Via Colinas
(between Via Rocas and Thousand Oaks Boulevard)**

Via Colinas (between Lindero Canyon Road and Via Rocas)

Via Colinas is the main street through the North Business Park. Between Lindero Canyon Road and Via Rocas, Via Colinas is a 4-lane street with a two-way center turning lane and Class II bike lanes on each side. One side of the street has an 8-foot wide sidewalk, and the other side of the street an 8-foot wide sidewalk separated from the street with a 4-foot wide planted parkway. On both sides of the street, trees planted every 30 feet and pedestrian streetlights approximately every 60 feet provide shade and lighting for a comfortable pedestrian environment. Benches and trash receptacles placed regularly will add convenience for pedestrians and cyclists. The recommended street tree for Via Colinas is the pedestrian-scaled Chinese Flame Tree, which will provide seasonal change, color, and shade.

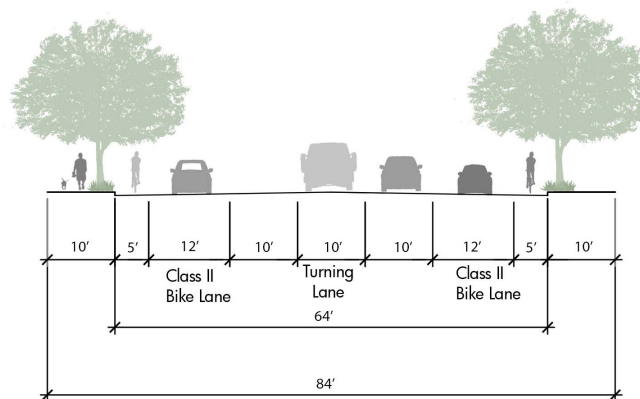
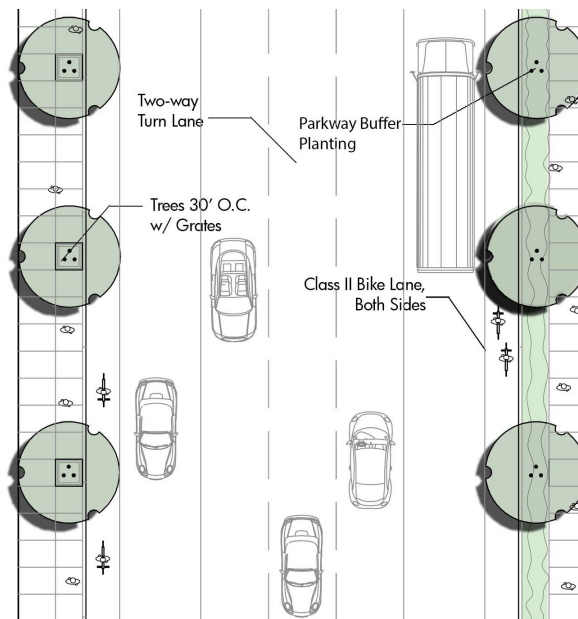


Figure 7-5: Via Colinas
(between Lindero Canyon Road and Via Rocas)

Via Rocas

Via Rocas is a two-lane street with parking on both sides and a central turning lane. On one side of the street, a 10-foot wide sidewalk is accompanied by 5-foot wide planted tree wells every 30 feet. On the other side, a 10-foot wide decomposed granite pathway gives plenty of room for pedestrians, with trees surrounded by tree grates every 30 feet. Class II bike lanes will be added on both sides. Standard overhead lights are recommended to provide sufficient light for the occasional heavy vehicular traffic caused by events at Oaks Christian School. Benches and trash receptacles will add convenience and comfort day and night. The recommended street tree for Via Rocas is the pedestrian-scaled Chinese Flame Tree, which will provide seasonal change, color, and shade, and continue to the Cedarvalley Drive cul-de-sac.

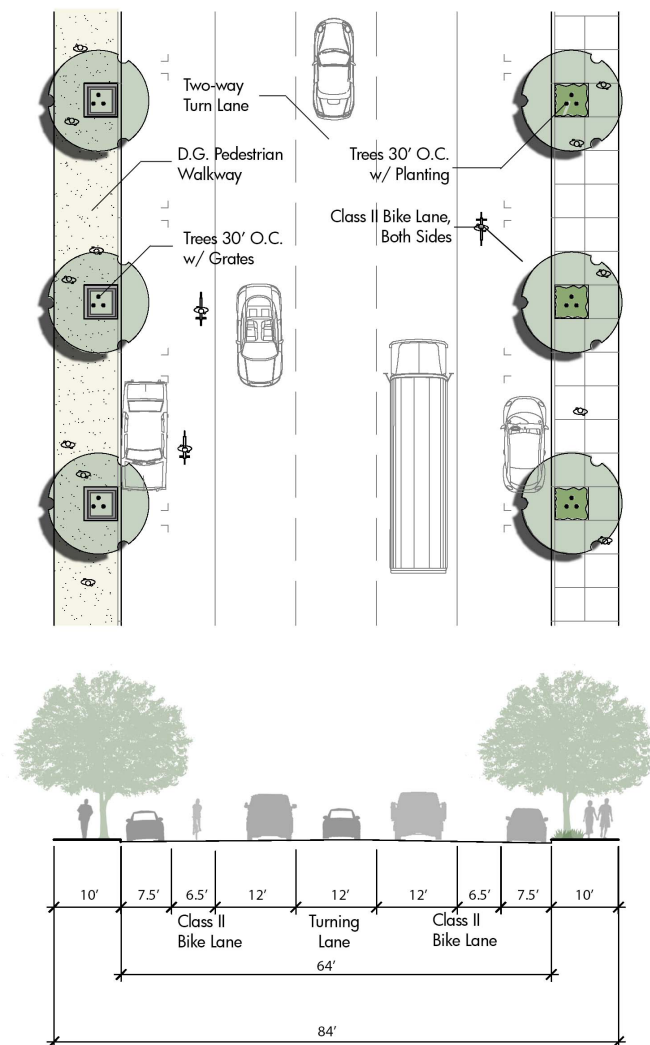


Figure 7-6: Via Rocas

La Tienda Drive

La Tienda Drive will have a new 10-foot wide sidewalk or decomposed granite pathway to accommodate pedestrians. Parallel parking lanes and class II bike lanes will be added to both sides of the street. Trees planted every 30 feet along the street will provide shade and give the street a distinct identity. The landscape buffer on the south side of the street along the freeway will remain. Standard overhead lights are recommended to provide sufficient light for the occasional heavy vehicular traffic caused by events at Oaks Christian School. Benches and trash receptacles should be placed at transit stops and major intersections. The Italian Stone Pine is recommended for La Tienda Drive, which fronts the freeway, because of its ability to filter airborne pollutants.

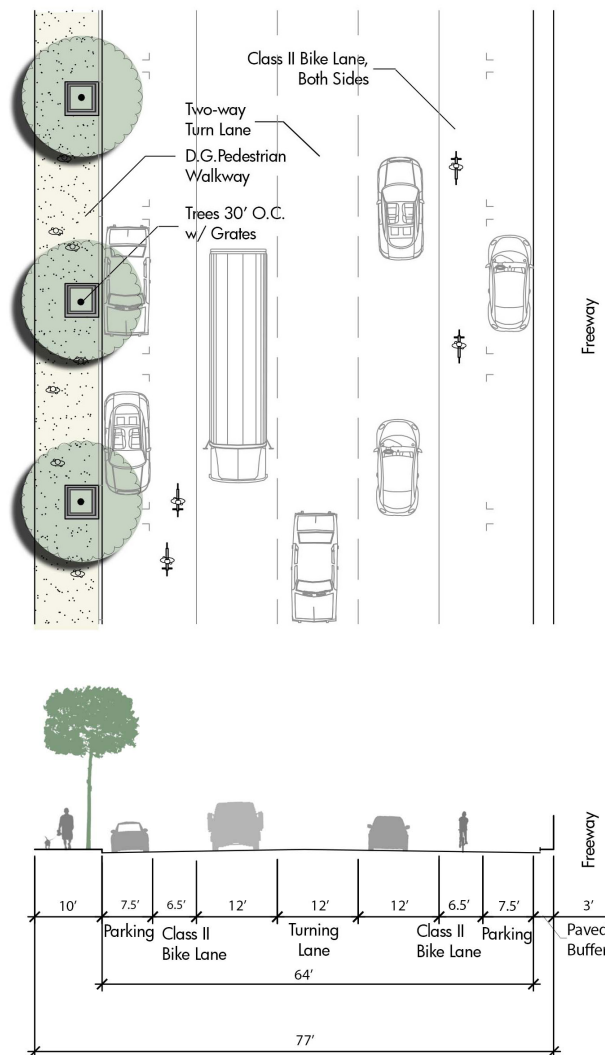


Figure 7-7: La Tienda Drive

La Baya Drive

La Baya Drive is primarily an east-west oriented roadway in the Specific Plan area and extends between Via Colinas and Thousand Oaks Boulevard. This two-lane street through the Design District is intended to become more pedestrian-oriented with new 5-foot wide sidewalks and a 5-foot planted parkway and shade trees approximately every 30 feet. Pedestrian lights approximately every 60 feet and streetlights approximately every 150 feet provide a safe and comfortable pedestrian environment day and night. The recommended street tree for La Baya is the London Plane tree, which is a large stately tree that will unify the street and provide shade for pedestrians.

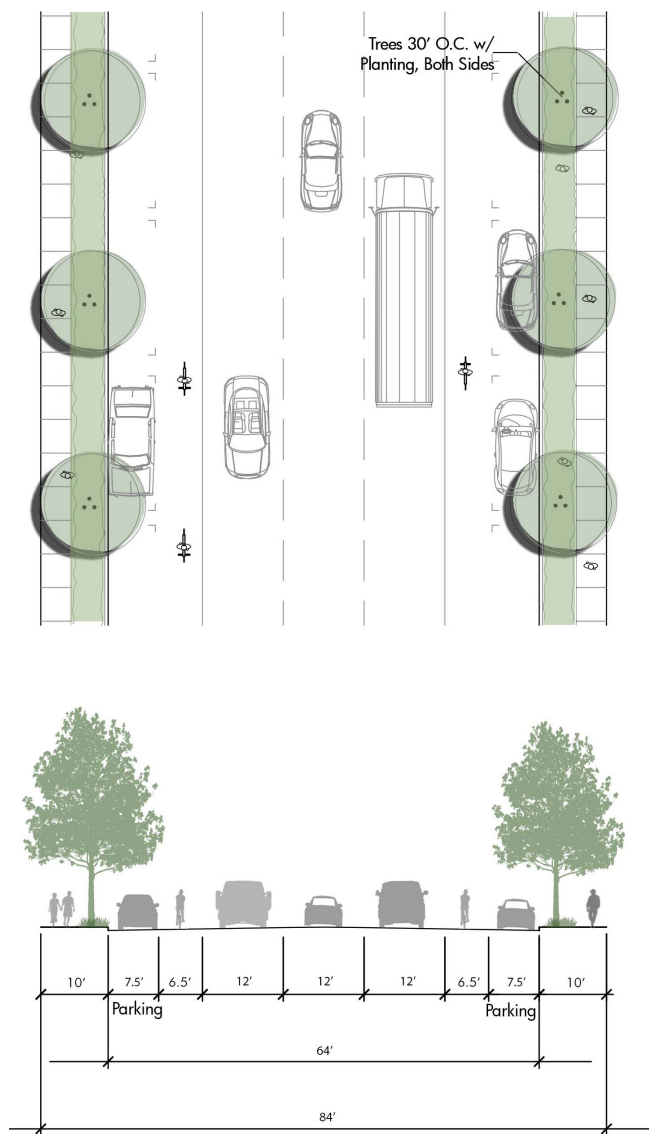


Figure 7-8: La Baya Drive

D. General Design Guidelines for Public-Rights-of -Way

1. Green Streets and Low Impact Development (LID) strategies, such as the use of vegetated swales and decomposed granite, shall be utilized as a natural systems approach to manage stormwater, improve water quality, reduce flows and enhance watershed health.
2. Sidewalks shall be graded to allow stormwater run-off into adjacent unpaved areas, such as planter strips and parkways.
3. Sidewalks shall comply with all ADA standards.
4. Street treatments in which stormwater runoff is captured and retained shall be located within street medians and parkways. Stormwater Best Management Practices per the California Stormwater Quality Association's Stormwater Best Management Practice Handbook shall be applied.
5. The Street Tree Matrix (Table 7-1) establishes the plant palette for the Specific Plan area. All plant material shall meet the minimum standard of the American Nurserymen and Landscape Association and California State Department of Agriculture Regulations, and be local container or field grown material.
6. All trees shall be planted in accordance with established City planting standards.
7. The minimum planting size for a parkway tree is a 24-inch boxed container.
8. Median and parkway trees shall be trimmed to retain a trunk space clear of branches of at least 7 feet from grade at maturity.

E. Street Furniture

A new palette of site furnishings that includes benches, trash receptacles, tree grates, and lighting will help define the Specific Plan area's emerging character and give it a coherent look and feel. While this Specific Plan does not identify specific furniture selections the following criteria should be used when selecting a street furniture palette:

- Street furniture should be selected to bring comfort, scale and design expression to the streetscape.
- The City's climate and setting should be taken into account in furniture selection, including colors and materials that are heat resistant.
- Durable street furniture with a modern sensibility is appropriate for the Specific Plan area; it should be highly durable and easy to maintain.
- All elements of the furniture palette should have a uniform look even if they come from different manufacturers and vendors; for example, a non-reflective grey powder-coat finish will tie the palette together. Light colored concrete furniture elements will also coordinate well together and require minimal maintenance.
- All furniture selected should discourage their use for skateboarding obstacles or for sleeping.

Lighting is also a key component that promotes safety and helps to create an appealing walkable environment. Lighting provides spatial definition to the sidewalk, adds ambiance to neighborhood settings and affords a sense of security to users. The selected light fixtures shall adhere to guidelines set forth by the Dark Sky Association to protect the area's view of stars and the needs of nocturnal wildlife. Light fixtures in the public right-of-way shall also follow the SCE standards for maintenance.



Examples of street furniture appropriate for the North Business Park. A consistent palette of street furniture will help unify the area and enhance the pedestrian experience.



Infrastructure Improvements

A. Introduction

The purpose of this chapter is to summarize the existing and planned infrastructure facilities and provide recommended infrastructure upgrades for the Westlake Village Business Park Specific Plan area. The recommended upgrades are based on comparing the existing facilities and their capabilities with the proposed development within the Specific Plan area. An analysis was conducted on existing infrastructure facilities in the Specific Plan area in “Infrastructure Analysis (Task 1.2.5) – Westlake Village, California, Business Park Specific Plan Area” (JMC², May 27, 2010).

B. Water System

The Las Virgenes Municipal Water District (LVMWD) is the water service provider for the North Business Park Specific Plan area. LVMWD owns and maintains the entire water supply network for both the potable and recycled water systems. There are potable water lines under all major streets in the business park area. The sizes of the potable water lines vary from 5 to 16 inches. Most of the pipes are ACP (asbestos cement pipe) with the exception of some steel pipes. The recycled water lines only exist on La Baya Drive, Lindero Canyon Road, Via Colinas, Via Rocas, and the area west of Via Colinas (the industrial park). The sizes of the recycled water lines are from 5 to 20 inches and most of the pipes are PVC (Polyvinyl chloride). Figure 8-1 shows the layout of both the existing potable water and reclaimed water lines.

There are no major backbone infrastructure upgrades required for the water facilities in order to meet the demand of the Specific Plan; however, it should be noted that LVMWD has future plans for upgrading the water infrastructure. A report and master plan for water system in the entire LVMWD district was prepared in 2014. The study examined the ability of the existing facilities to adequately meet the water demands then and for the next 25 years. In the report titled “Integrated Water System Master Plan Update 2014” (Kennedy/Jenks, 2014), multiple projects were proposed in order to cope with the demand for the future build-out of the entire district. Two of the

projects involved improvements within the Specific Plan. Both projects, if constructed, will be funded by LVMWD. These projects are as follows:

1. **Calleguas-LVMWD Interconnection:** Transmission system improvements are needed for transferring surplus water supply from Calleguas Municipal Water District (MWD), which is north of the Specific Plan area, during the winter months to the Las Virgenes Reservoir, which is south of the Specific Plan area. The Calleguas-LVMWD Pipe line from south side of the 101 Freeway along Lindero Canyon Road to Kanan Road has been upgraded to a 30-inch pipe. The pipe will extend up Lindero Canyon from Thousand Oaks Blvd to the County Line. Calleguas MWD will provide a pump station and interconnect to this line. This project provides additional water supply for the Las Virgenes reservoir storage during the summer months, not only for the Specific Plan area but the entire water district.
2. **Thousand Oaks Boulevard Extension:** A 12-inch Reclaimed Water Main was installed down Thousand Oaks Blvd to Westlake Village Community Park until in 2009. The line is approximately 1,070 ft. There are no future plans to extend it any farther.

C. Sewer System

The sewer lines serving the Specific Plan area are owned by the City of Westlake Village and maintained by the Los Angeles County Department of Public Works (LACDPW). The major trunk lines and treatment plants in the area are owned and maintained by the Las Virgenes Municipal Water District (LVMWD). The sewer lines within the area are mainly Vitrified Clay Pipes (VCP) with the sizes ranging between 8 and 18 inches. The sewer lines are under major streets within the Business Park, with smaller laterals connecting directly to the businesses. As it flows south toward the LVMWD trunk sewer, the City sewer main traverses through easements and along Lindero Channel Access road (Figure 8-2).

The recently monitored flows in the City's sewer main is nearing design capacity, as the contributing areas are currently built out. There is some remaining capacity for intensifying the uses as proposed in the Specific Plan, however it is limited.

Any research and study about the capacity to handle future demand will be conducted by the developers and reviewed by LVMWD and the City. When future developers are ready to submit proposed preliminary design plans to the City, they will be required to conduct an individual sewer area study to determine whether upgrades are necessary. Identified upgrades to the sewer system, if any, will be required to be designed and implemented with each development.

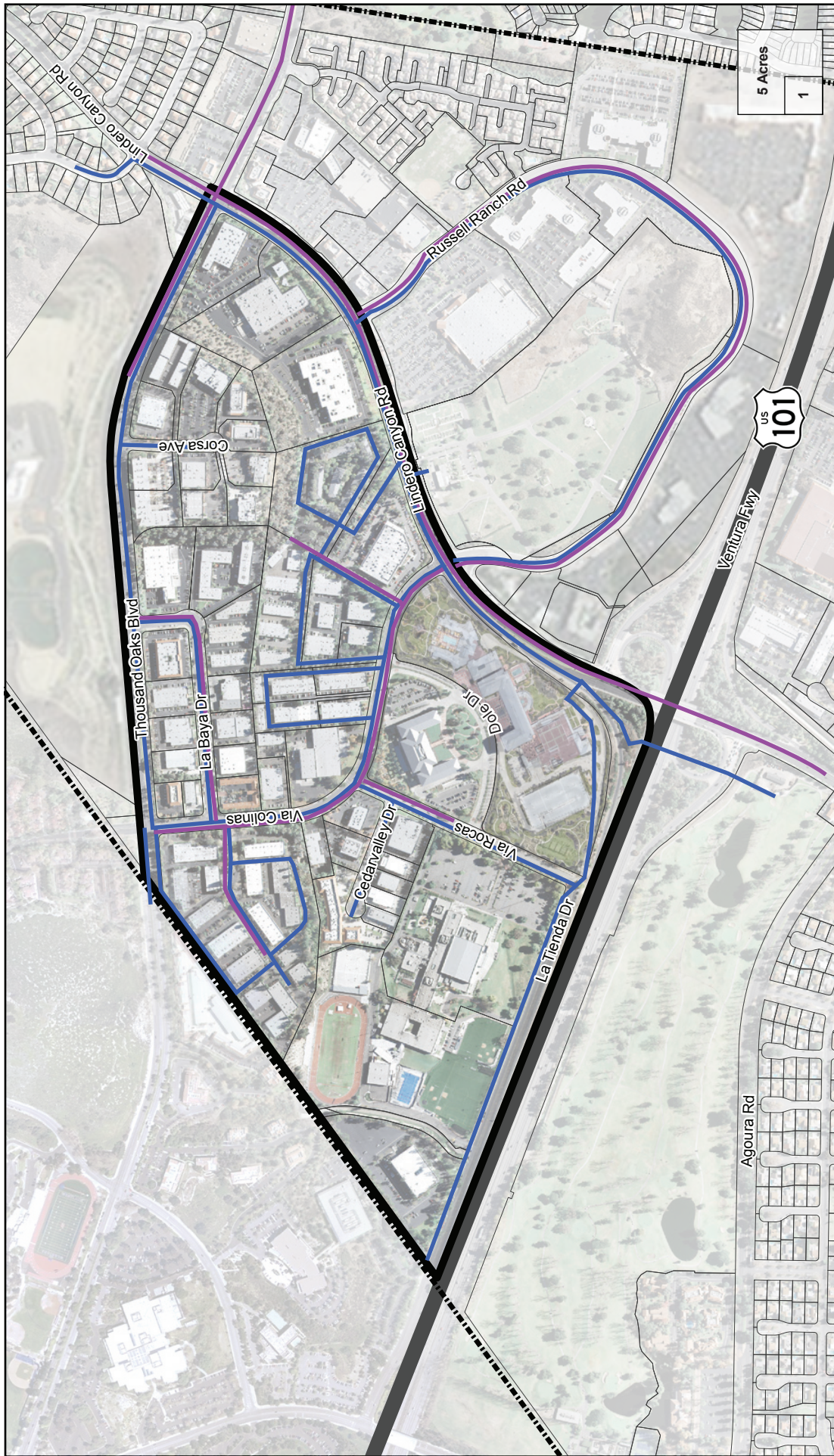


Figure 8-1:

Water System Infrastructure

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



Specific Plan Area

City Boundary

Potable Water Main Lines

Recycled Water Main Lines

D. Storm Drain System

The storm drain system serving the Specific Plan area is owned and maintained by the Los Angeles County Flood Control District (LACFCD). The storm drain system within the Specific Plan area consists of mostly underground Reinforced Concrete Pipes (RCP) ranging from 18 to 78 inches under the major streets with the exception of a 26-foot wide open channel drain on the southwest side of the Specific Plan area at the back of the Oaks Christian School. This open channel drain directs the storm water runoffs southward across La Tienda Drive and the Ventura Highway (101 Freeway) via a culvert to another open channel along the south side of the freeway.

Future development in the Specific Plan area will generate little or no increase in the runoff to the existing drainage system, since more than 90% of the existing Specific Plan area is already impervious. It is predicted that the new development will not directly trigger any need for upgrading LACFCD's existing storm drain major backbone facilities. In addition, the requirements for percolation and on-site detention for new development will stabilize and/or even reduce runoff in the area.

Similar to the sewer system, research about the current capacity and any need for future upgrades of the drainage system will only be conducted by the developers and reviewed by the City and LACFCD when the proposed preliminary design plans are submitted. At the present time, there are no plans to upgrade the existing storm drain system within the Specific Plan area.

LID Practices and Project Requirements

Since January 20, 2005, the State Water Resource Control Board of California (SWRCB) adopted a sustainable practice called Low Impact Development (LID) that would benefit the water supply and contribute to water quality protection for the region. Unlike traditional storm water management, which collects and conveys storm water runoff through storm drains, pipes, or other conveyances to a centralized storm water facility, LID takes a different approach by using site design and storm water management to maintain the site's pre-development runoff rates and volumes. The goal of LID is to mimic a site's pre-development hydrology by using design techniques that infiltrate, filter, store, evaporate, and detain runoff close to the source of rainfall.

The ten LID practices are:

- 1) Bioretention & Rain Gardens
- 2) Rooftop Gardens
- 3) Sidewalk Storage
- 4) Vegetated Swales, Buffers & Strips; Tree Preservation
- 5) Roof Leader Disconnection
- 6) Rain Barrels and Cisterns

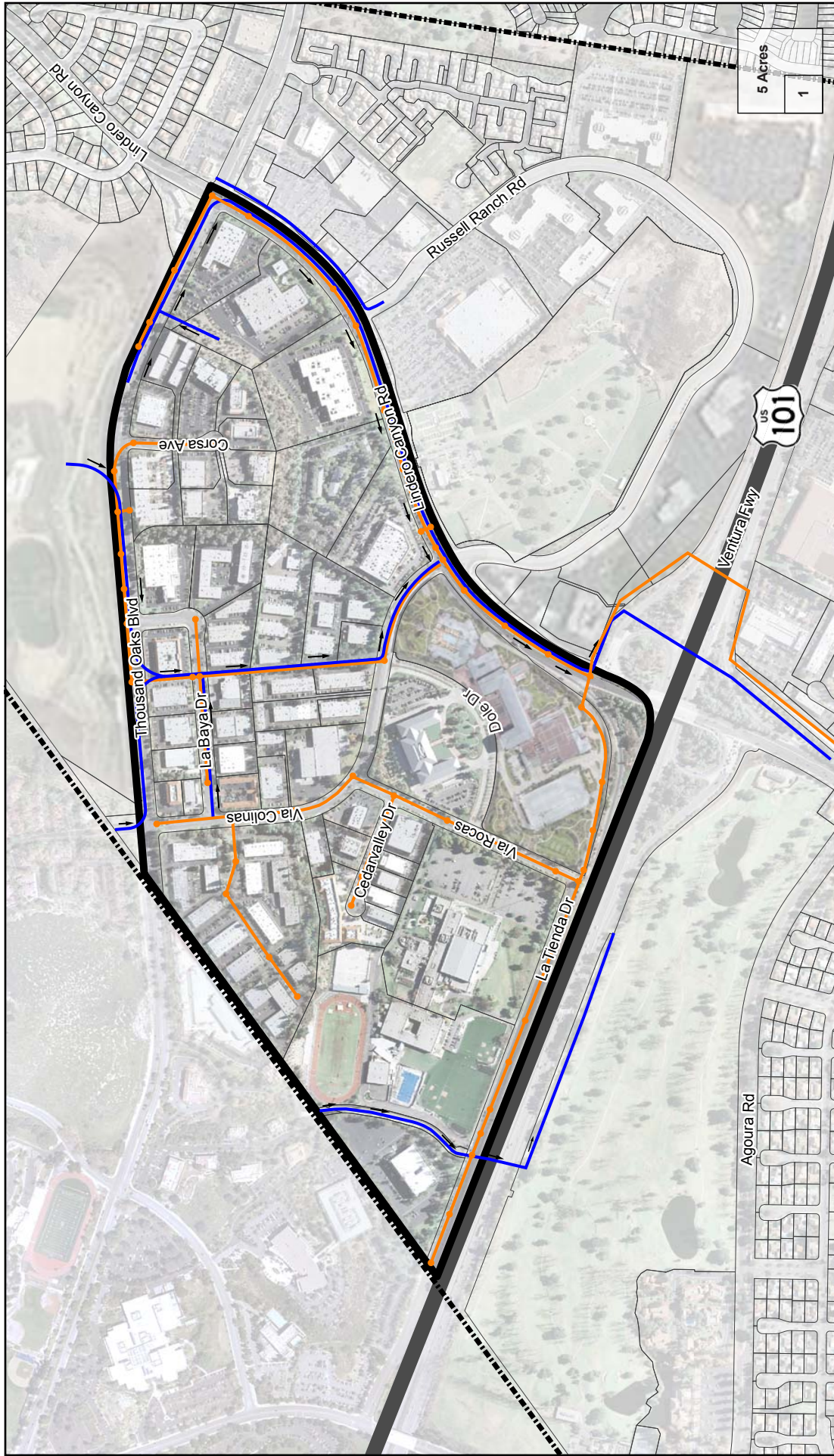
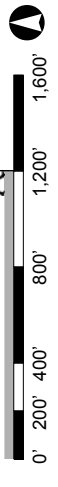


Figure 8-2:

Sewer and Storm Drain Infrastructure

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



- Specific Plan Area
- City Boundary
- Existing Sanitary Sewer
- Existing Storm Drain
- Existing SD Flow Direction

- 7) Permeable Pavers
- 8) Soil Amendments
- 9) Impervious Surface Reduction & Disconnection
- 10) Pollution Prevention & Good Housekeeping

All new development in the Specific Plan area will require preparation of a hydrology study to demonstrate that building sites are free from flooding hazard. All new development will be required to mimic the site's pre-development runoff by choosing the appropriate LID practice most suitable for the site. A proposed project must demonstrate that any proposed improvement, including filling, does not raise the flood level upstream or downstream of the project. In addition, National Pollution Discharge Elimination System (NPDES) reports, such as the Stormwater Mitigation Plan and Storm Water Pollution Prevention Plan (SWPPP), will be required by the City and State of California from the developer for each new development to ensure the quality of water is preserved and adverse environmental impacts are minimized.

E. Electrical System

Southern California Edison (SCE), an independently owned utility, provides electrical power service to the City of Westlake Village. SCE sets their own service standards (with the involvement of the Public Utilities Commission) and facility improvement strategies. Currently, there is a network of power grid lines which supply sufficient electrical power service to the Specific Plan area. There is no major deficiency or functional problem in the power supply facilities within the Specific Plan area.

With the coordination of the City, the decision to upgrade the power supply facilities and the quantities of the upgrade (if any) in order to meet the demand of future development will be decided by SCE after developers have submitted their building plans. Demand for services and the ability to serve new developments are generally determined on a case-by-case basis. At this point, there are no recommendations for any major upgrades to the existing power supply facilities in the Specific Plan area.

It is important for developers to note that SCE has developed several energy-efficiency programs for residential, non-residential, new construction and low-income subscribers. These programs include rebates and cash incentives for completion of energy-efficiency projects in residences and businesses, providing energy-efficient solutions for new developments as well as programs that aid low-income customers to purchase energy-efficient refrigerators and outdoor lighting. SCE will continue to promote the resourceful use of energy, and in turn, a reduction in electrical use and electricity. Most new development will be required to place the power and telephone lines underground.

F. Natural Gas System

The Southern California Gas Company / Sempra Utilities (The Gas Company) is the gas service provider for the Specific Plan area. Currently the gas pipelines are in all major streets in the area, except for some portions of Thousand Oaks Boulevard. Specific locations of gas pipelines are illustrated in Figure 8-2 and pipe sizes are identified in Table 8-1.

Because The Gas Company is an independent private entity, the analysis on the capacity and capability to meet future demand will be conducted by The Gas Company with the coordination with the City upon submittal of building plans by developers. It is important for developers to note that The Gas Company participates in the California Energy Star® New Homes Program, a performance-based program that provides builders with incentives for developments that use at least 15% less energy than standards set forth in the California Energy Efficiency Standards.

Table 8-1: Pipe Sizes of Existing Gas Lines

Locations	Pipe Sizes
La Tienda Road from County border to 527' east of the border line	4"
The remainder of the gas line under La Tienda Road	3"
Via Rocas	3"
Cedarvalley Drive	3"
Via Colinas west of Via Rocas intersection	4"
Via Colinas east of Via Rocas intersection	3"
Thousand Oaks Boulevard west of Via Colinas intersection	6"
Thousand Oaks Boulevard from La Baya Dr. intersection to Corsa Ave. intersection	4"
La Baya Drive	2"
Corsa Avenue	2"
Lindero Canyon Road	3"

G. Telecommunications System

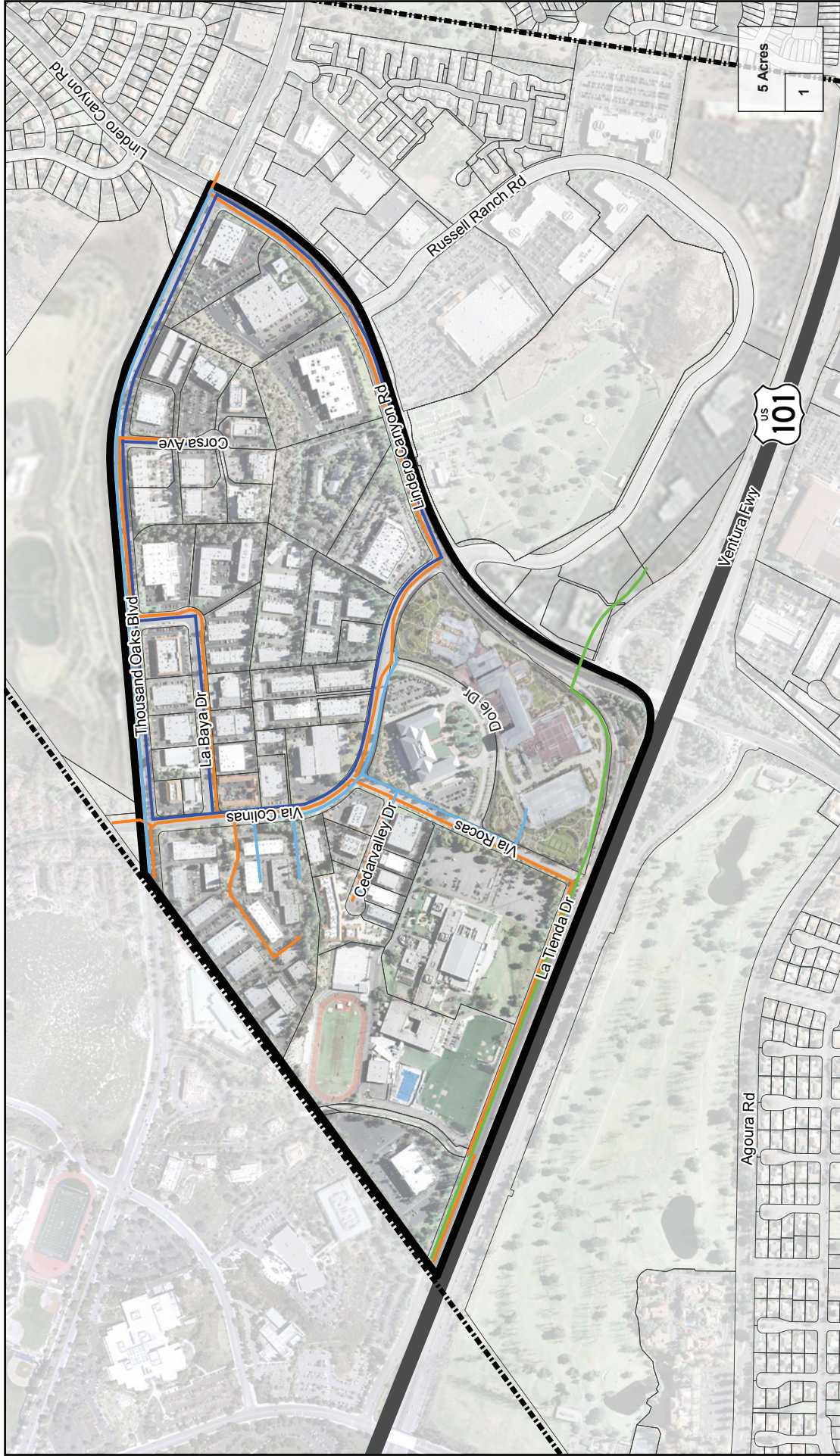
The City of Westlake Village is within the service area of AT&T, a privately owned company. They are the provider of both local and long distance telecommunications and internet service in the Specific Plan area.

AT&T will assess the demand for services and ability to serve new developments on a case-by-case basis. The capacity and capability analysis for meeting future demand within the Specific Plan area will be conducted after building plans are submitted by developers. An upgrade of the existing telecommunications infrastructure to have the capability to provide high speed internet to future developments will need to include a fiber optic cable backbone to be added on the major streets within the Specific Plan area (see Section H, Cable Television System). The new fiber optic cable will have the capability of providing high speed internet via the cable provider.

H. Cable Television System

The cable service provider for this area is Spectrum. Most of the Specific Plan area does not have cable services. Specifically, the triangular “pocket” formed by Via Colinas, Thousand Oaks Boulevard and Lindero Canyon Road has virtually no underground coaxial cables. The businesses that have cable services are those on the south west side of Via Colinas, including the Dole Corporate Headquarters and Four Seasons Hotel.

New land use resulting from the Specific Plan will consist of both commercial and residential development, therefore high speed internet and cable television services will be in demand. A fiber optic cable backbone will be required to be added in the major streets within the Specific Plan area. Figure 8-2 illustrates the recommended layout of the proposed fiber optic line. Since Southern California Edison (SCE) already has an existing power supply network in this area, joint trenching of coaxial cable for new businesses or residential units will not be an option. Therefore, direct trenching or subsurface borings will most likely be required.



Specific Plan Area

City Boundary

Existing Gas

Existing Cable TV (Coaxial)

Existing Telephone

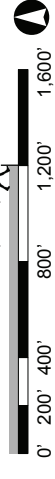
Planned Cable TV (Fiber Optic)

Figure 8-3:

Existing and Planned Dry Utilities

NORTH BUSINESS PARK SPECIFIC PLAN

5 minutes walk (1,200')



Economic Implementation Strategy

A. Introduction

The Specific Plan identifies a number of landscaping, streetscape and circulation elements that are critical to achieving the vision for the Specific Plan area as the city's first high density mixed use neighborhood. In order maintain the City's strong fiscal position; the Specific Plan Economic Implementation Strategy seeks to balance public and private investment in the landscaping, streetscape and circulation elements critical to the successful transition of the Specific Plan area.

Preliminary one-time capital costs are estimated for streets and roads, landscaping, and other infrastructure required for full build-out of the North Business Park Specific Plan. A range of enhanced operations and maintenance costs are also estimated. Some of these costs are proposed to be financed by development impact fees and others are proposed through the use of assessments or a Property Based Business Improvement District (PBID). These preliminary cost estimates would need to be refined once the Specific Plan is approved.

As part of the Specific Plan process, a financial feasibility analysis was completed to determine: 1) that the land uses considered integral to the community's future revitalization were financially feasible from a developer or landowner perspective; and 2) to determine the ability of developers, landowners, and tenants to contribute financially to the redevelopment and revitalization of the area through additional development impact fees and assessments.

The financial feasibility analysis concluded: 1) long-run market conditions are generally favorable for redevelopment and revitalization; and 2) there is limited market capacity for a combination of assessments and fees to offset some of the public capital costs and local maintenance envisioned in the Specific Plan.

However, absent an above average improvement in long-run market and transportation conditions, higher density development will be less probable than traditional lower

density development. Generally, market conditions in the greater Conejo Valley are not strong enough to absorb the full cost of the structured parking necessary to achieve some of density objectives contemplated in the preferred plan without some cost sharing from the public sector.

Successful redevelopment and revitalization of the Specific Plan area will rely on a combination of economic development strategies and land use regulations that may transform the business park into a thriving mixed-use neighborhood. Interesting and identifiable place-making often leads to increased investor interest and superior real property financial performance, which improves the probability of achieving the community vision developed in the Specific Plan.

The implementation measures identified in this strategy are intended to guide City staff, property owners, developers and decision-makers to ensure that an adequate system of infrastructure and enhanced amenities is in place for future development and to enhance the long-term financial stability and fiscal feasibility of the North Business Park Specific Plan.

B. Opportunities and Constraints

Typical of most mature communities in Los Angeles, the North Business Park has relatively little vacant land or open space available in its commercial corridors and industrial areas. The lack of vacant land plays an important part in the feasibility analysis. Virtually all meaningful forms of redevelopment in the commercial zones will require some new construction and land assemblage.

Also, as development and density increases, transportation management will become increasingly important for both on-site parking as well as for transit, shuttle and pedestrian and biking access. On-site parking management will need to find solutions that allow smaller and medium-sized parcels to contribute their fair-share for parking solutions through in-lieu fees and direct payments to larger developments, which may provide additional parking facilities and shared parking solutions that can serve smaller and medium-sized parcels, along with their own needs.

C. Infrastructure Financing Strategy

The infrastructure financing strategy is designed to ensure that an adequate infrastructure system is in place for future development in the North Business Park:

- Require new development to contribute its fair share of the cost of on- and off-site public infrastructure, landscaping, sidewalks, enhanced amenities and transportation management solutions.
- Consider innovative financing mechanisms, including, but not limited to, establishing Community Facilities Districts (CFDs), Special Assessment Districts, Development Impact Fees and participation in a Capital Improvement Program (CIP) to fund and maintain enhanced local public facilities and infrastructure.
- Based on final capital cost estimates, establish development impact fees for new development's fair share cost of required capital facilities which could alternatively be paid through a master assessment district.
- Apply for available State, Federal and regional funding sources to finance public infrastructure and transportation improvements costs.
- A parking district through a transportation management plan has the potential to serve as a catalyst for redevelopment in the Specific Plan area. It is recommended further study be given to the preparation of such a management plan that balances the development goals of the Specific Plan area with the stakeholder's and city's ability to fund the necessary improvements.

D. Economic Implementation Actions

Implementation of a financing and operations and maintenance plan ensures that new development will meet the service level specifications identified in the Specific Plan and that new development pays its fair share of the backbone infrastructure, including other public facilities and enhanced amenities required to serve the Specific Plan area. While individual areas of the Specific Plan may develop at different times as the financing plan is implemented, the available funding sources will change as development occurs. Therefore, the financing plan will need to be updated periodically as modifications to financing programs, land uses, and cost estimates for infrastructure and public facilities occur. The financing plan should be re-evaluated within the context of the overall economic strategy to ensure required funding is available when needed. The costs and funding sources will also need to be adjusted periodically to reflect inflation impacts over time. In summary, possible changes to the financing plan include:

- New or revised infrastructure projects;
- New construction cost information or engineering estimates, including inflation; and
- New funding source data.

Also, the implementation strategy ensures that new development will cover its operations and maintenance costs either through locally generated public revenues or through the application of various financing techniques to cover any ongoing deficits when other public revenues are not sufficient. This may include various combinations of techniques to cover ongoing operations and maintenance costs, such as landscape and lighting districts, Community Facilities District (CFD) special taxes, and a property owner's business improvement district (PBID).

1. Implementation Strategy Steps

The North Business Park property and business owners, in partnership with the City staff and officials, will be taking on new responsibilities in managing, guiding and facilitating the orderly development of the North Business Park Specific Plan area in order to realize a financially and fiscally sound community that will assist the City Planning staff and lead to the cost-effective delivery of public services, as summarized in Table 9-1.

a) Administrative

- Form a committee of City, North Business Park property and business owners, and development representatives.
- Initiate a petition to prepare a business improvement district with property and business owners.
- Identify the responsible agency or entity for each infrastructure or service category.
- Prepare a set of public revenue performance indicators to monitor the ongoing fiscal health of the North Business Park Specific Plan.
- Update the City's Capital Improvement Program (CIP) to reflect the Business Park's new public infrastructure and enhanced amenities, as well as the financing strategy.

b) Infrastructure Financing

- Finalize the cost estimates of the infrastructure to be paid for by new development.
- Based on the approved land uses, prepare an assessment engineer's report that will establish the approved spread of infrastructure costs among approved land uses and the development impact fees.
- For transportation development impact fee:
 - Identify any revenues from regional or sub-regional transportation fee programs already established;
 - Spread the net transportation costs on a trip related methodology;
 - Estimate the fair share amount by land use; and

- Adopt a development impact fee (DIF) schedule through the City of Westlake Village with input from North Business Park stakeholders.
- For landscaping and lighting improvements along arterials and internal streets and roads:
 - In order to allow for the timely provision of landscape and lighting improvements, identify the appropriate financing mechanism, such as a landscape and lighting district (LLMD) assessment;
 - Establish the LLMD with a majority landowner vote over the benefiting properties with the Specific Plan area;
 - As facilities are required, use LLMD bonding authority can also provide for landscape and lighting infrastructure financing; and
 - Provide for reimbursement program to developers who finance necessary infrastructure beyond their fair share.
- For other facilities that serve the entire Specific Plan area, such as undergrounding fiber optic cable for TV and all forms of data transmission:
 - Establish an assessment that will spread the costs on a fair share basis to the respective properties;
 - Provide for reimbursement program to developers who finance this cost beyond their fair share; and
 - For properties that have general benefit from improvements beyond the Specific Plan boundaries, such as transportation and road improvements, consider other funding sources such as outside grants and loans or consider City contributions.

c) Provide for Expanded and Shared Parking Structures

- Identify locations where shared parking between the public and private sectors will both lower costs to the private sector and incentivize desired development and provide public benefits to off-site recreational and community activities.
- Develop a transportation management plan that will facilitate the development and maintenance of structured parking over time using a variety of funding sources, including: development impact or in-lieu fees, special assessments, parking revenues and public contributions and incentives, particularly through outside grants and loans.

d) Maintain Fiscal Balance

- In order to cover the enhanced costs of landscaping and other operations and maintenance costs on a fair-share fiscal basis, establish an annual special assessment.
- Encourage commercial retail uses, particularly in the Design District, to generate new sales tax revenue to help offset ongoing enhanced operations and maintenance costs from residential and non-residential growth.
- Provide annual monitoring of the fiscal balance of the North Business Park.

Table 9-1: Implementation Strategy Plan for the North Business Park Specific Plan

Implementation Step	Key Actions	Responsible Parties	Phasing Priority
Update the City's Capital Improvement Program	<ul style="list-style-type: none"> Update the City's Capital Improvement Program that includes a long-term financing strategy comprised of a variety of local and outside funding mechanisms for the proposed improvements and enhanced amenities 	City of Westlake Village, Local Stakeholders	High
Foster Public-Private Partnerships	<ul style="list-style-type: none"> Coordinate funding strategies between the City's Planning and Engineering Departments with a local Property Based Improvement District (PBID) 	Planning and Engineering Departments, PBID	High
Update the Development Impact Fee (DIF) Schedule for the Specific Plan area	<ul style="list-style-type: none"> Review the existing development impact fees for the Specific Plan area to reflect new development's equitable share of the proposed public improvements and enhanced amenities 	Planning and Engineering Departments	High
Explore Interest in Establishing a PBID	<ul style="list-style-type: none"> Prepare a feasibility study to gauge property owner interest in establishing a PBID, and identify the program costs for the proposed district as well as an equitable formula to establish and allocate future assessments 	PBID Stakeholders in coordination with City of Westlake Village	High
Landscape and Lighting District (LLD)	<ul style="list-style-type: none"> A landscape and lighting district could be established for the maintenance of landscaping, sidewalk and related enhanced public improvements; property assessments would be subject to a majority vote of the property owners 	Planning Department; Property Owners	High
Pursue Regional, State and Federal Infrastructure Funding Sources	<ul style="list-style-type: none"> Identify, monitor and apply for other government funding sources, for infrastructure, transportation, pedestrian and bicycle improvements 	City of Westlake Village; Regional, State and Federal Entities	Medium & Ongoing
Develop a Transportation Management Program	<ul style="list-style-type: none"> As development occurs on small, medium-sized and larger parcels, the issues of on-site parking, transit and shuttle services and facilitating walking and biking will need long-term transportation management plan 	Public Works Department and Local Stakeholders	Medium
Marketing and Outreach	<ul style="list-style-type: none"> Develop a marketing and outreach program that would advertise the Business Park's revitalization efforts to the broader market area; this marketing program could be coordinated with the local Chamber of Commerce and a future PBID 	Planning Department, City Manager's Office, Chamber of Commerce, PBID	High
Monitor Fiscal Performance of Increase in Specific Plan Public Revenues	<ul style="list-style-type: none"> Set up a monitoring system of indicators to track public revenues, employment, population and housing growth 	Planning Department	Medium & Ongoing

¹These phasing priority periods are identified, including; High (1-5 years), Medium (5-10 years), and Low (10+ Years); ongoing means occurring throughout the phasing period.

